

AGENDA
WORKSHOP MEETING – BOARD OF COMMISSIONERS
FEBRUARY 7, 2022 – 7:00 PM

ZOOM MEETING ID: [838 0240 0102](https://us02web.zoom.us/j/83802400102)
LINK TO LIVE BROADCAST: <https://us02web.zoom.us/j/83802400102>

As of July 1, 2021, Springfield Township has resumed in-person meetings for all public meetings. Public comment is limited to in-person participants or comments submitted in advance of the meeting.

Effective Monday, August 9, 2021, face masks are REQUIRED to be worn at all times inside Township buildings and facilities.

1. **Library Advisory Committee Membership**: (a) consider the appointment of a school district representative to fill an unexpired term of service on the committee, and (b) review the resumes of two residents interested in serving on the committee
2. **Land Development – Flourtown Shopping Center** – construction of a 2,450 sq. ft. pad site - accept a letter granting an extension to the 90 day land development plan review period
3. **Land Development – Flourtown Swim Club** – accept a letter granting an extension to the 90 day land development review process
4. **Land Development – LaSalle College High School** – request to waive the formal land development review process to convert two athletic fields to synthetic turf
5. **Planning Commission** – consider the reappointment of two existing members
6. **Open Space Plan** – consider the process by which the Township’s existing open space plan might be updated
7. **Recycling Report** – review the monthly recycling activities
8. **Plastics Recycling** – review the costs associated with conducting a special drop off collection for plastic materials that are not accepted in the curbside collection program
9. **Clean Energy Transportation Plan** – review and provide comment on a draft plan created by the Environmental Advisory Commission
10. **Bid – school zone flashers** – Paper Mill Road and Montgomery Avenue – review bid results and consider awarding a contract for the installation of school zone flashers

11. **Sale of Used Equipment** – authorize the sale of a used zero-turn mower via the Municibid on-line auction site
12. **Integrity Avenue Stormwater Improvements** – review the benefits of a proposed stormwater improvement project
13. **Walnut Avenue Pedestrian Trail** – consider preparation of plans and specifications for phase I of the subject trail, and authorize the submission of grant applications for phase II
14. **Township Manager’s Report** – discuss operational issues as outlined in the monthly report
15. **February Bill Listing** – review and approve the monthly bill listing and check reconciliation of the previous month
16. **Zoning Hearing Board Agenda** – announce the agenda of any special or regular meeting of the Zoning Hearing Board
17. **Flourtown Avenue** – review the results of a traffic study for the 8100 block of Flourtown Avenue
18. **Station Avenue and Church Road intersection** – review the crash statistics for the subject intersection and any recommendations provided by the traffic safety officer
19. **Parks and Recreation Advisory Committee Membership** – consider the appointment of a school district representative to fill an unexpired term of service on the committee
20. **Resolution No. 1561** – a resolution authorizing application to the Montco 2040 Implementation Grant Program
21. **Erdenheim Civic Association** – request to waive permit fees associated with three community events
22. **Little League Land Use Agreements** – consider approval of land use agreements with the Springfield baseball and softball Little League programs
23. **Springfield Little League** – consider a request to move the existing scoreboard from Marlow Field to Bysher Field

NEW BUSINESS

Michael Taylor
Township Manager

NOTE: AGENDA ITEMS ARE SUBJECT TO CHANGE WITHOUT FURTHER NOTICE!

ANY INDIVIDUAL WITH A DISABILITY WISHING TO ATTEND THE ABOVE SCHEDULED MEETING AND REQUIRING AN AUXILIARY AID, SERVICE OR OTHER ACCOMMODATION TO PARTICIPATE IN THE PROCEEDINGS, PLEASE

**CONTACT THE OFFICE OF THE TOWNSHIP MANAGER AT 215-836-7600, 72 HOURS
PRIOR TO THE MEETING.**

BYLAWS
FREE LIBRARY OF SPRINGFIELD TOWNSHIP
LIBRARY ADVISORY COMMITTEE

This Organization shall be known as the:
Free Library of Springfield Township Advisory Committee.

MEMBERSHIP:

The Library Advisory Committee will be comprised of seven (7) members and one (1) alternate member to serve in an advisory capacity to the Board of Commissioners. Members will be appointed by the Commissioners for three-year terms on a staggered basis. A representative of the School District of Springfield Township shall serve as a voting member of the Library Advisory Committee. The Township Commissioner liaison shall be responsible for providing communications to and from the Board of Commissioners and Library Advisory Committee, and be a voting member. A representative of the Friends of the Free Library of Springfield Township (a fundraising 501C-3 organization) shall be a voting member. When a member of the Committee has missed three consecutive meetings or four meetings annually, the Commissioners shall determine if the Committee member should be replaced.

ALTERNATE MEMBER:

If by reason of absence or disqualification of any regular member of the advisory committee, the President of the committee shall designate the alternate to sit as a voting member of the committee. When not seated as a voting member of the committee, the alternate member shall be entitled to participate in any discussion of the committee.

DUTIES:

The Library Advisory Committee will recommend to the Board of Commissioners the overall philosophy and goals of the library programs.

On an annual basis they will recommend to the Board of Commissioners objectives of the library program within the philosophy and goals previously established by the Board of Commissioners.

The Library Advisory Committee will serve as a sounding board for library needs of township residents and transmit those needs to the Library Director and Board of Commissioners for Board consideration and approval.

They will submit to the Board of Commissioners a recommended capital improvements program, and be advised of all plans affecting the library.

The Library Advisory Committee will promote public interest and understanding of the Free Library of Springfield Township and related philosophies of the Board of Commissioners.

The Library Advisory Committee members must have a current Library Card.

MEETINGS:

All meetings shall be held as public meetings. Advisory Committee meetings will be held bi-monthly five times per year from October through June on the fourth Tuesday of the month at 7 PM. A final meeting for the year will be held in June. There will be no scheduled meetings in July and August. Special meetings may be called by the President with consent by a majority of the committee members

OFFICERS:

Officers of the Advisory Committee shall be chosen at the Annual Year-end meeting of the Committee for a term of one year. If a vacancy occurs, a new officer shall be elected to fill the unexpired portion of that term. The office of Treasurer will be held by the Commissioner that is appointed by the board of commissioners.

The **President** shall preside at all meetings and appoint any committees and committee chairpersons necessary to conduct library affairs. The President's duty is to sign the Commonwealth Library Annual Report.

The **Vice-President** shall assume all the duties of the President in his/her absence.

The **Secretary** shall keep complete and accurate records of all meetings and actions of the Committee.

The **Treasurer's** duty is to sign the Commonwealth Library Annual Report.

QUORUM: A majority of the Advisory Committee shall constitute a quorum for the transaction of business. No proxy votes are permitted. Simple majority votes shall carry Committee meetings.

BYLAWS: Bylaws may be amended at any regular meeting of the Committee by a majority vote. The Board of Commissioners shall approve any by-law amendments.

Library Advisory Committee Vacancy

Residents sought to fill vacancy on Advisory Board



JAN 13, 2022

GENERAL NEWS ▾

Springfield Township is currently accepting resumes and letters of interest for one advisory board vacancy on the Library Advisory Committee.

The **Library Advisory Committee** is a volunteer group of residents appointed to serve in an advisory capacity to the Board of Commissioners on the overall philosophy and goals of the Free Library of Springfield Township and to serve as a sounding board for the library needs of the residents of Springfield Township.

The Springfield Township Library Advisory Committee meets on the fourth Tuesday of every other month (January, March, May, July, September, and November) at 7:00 p.m in the Library.

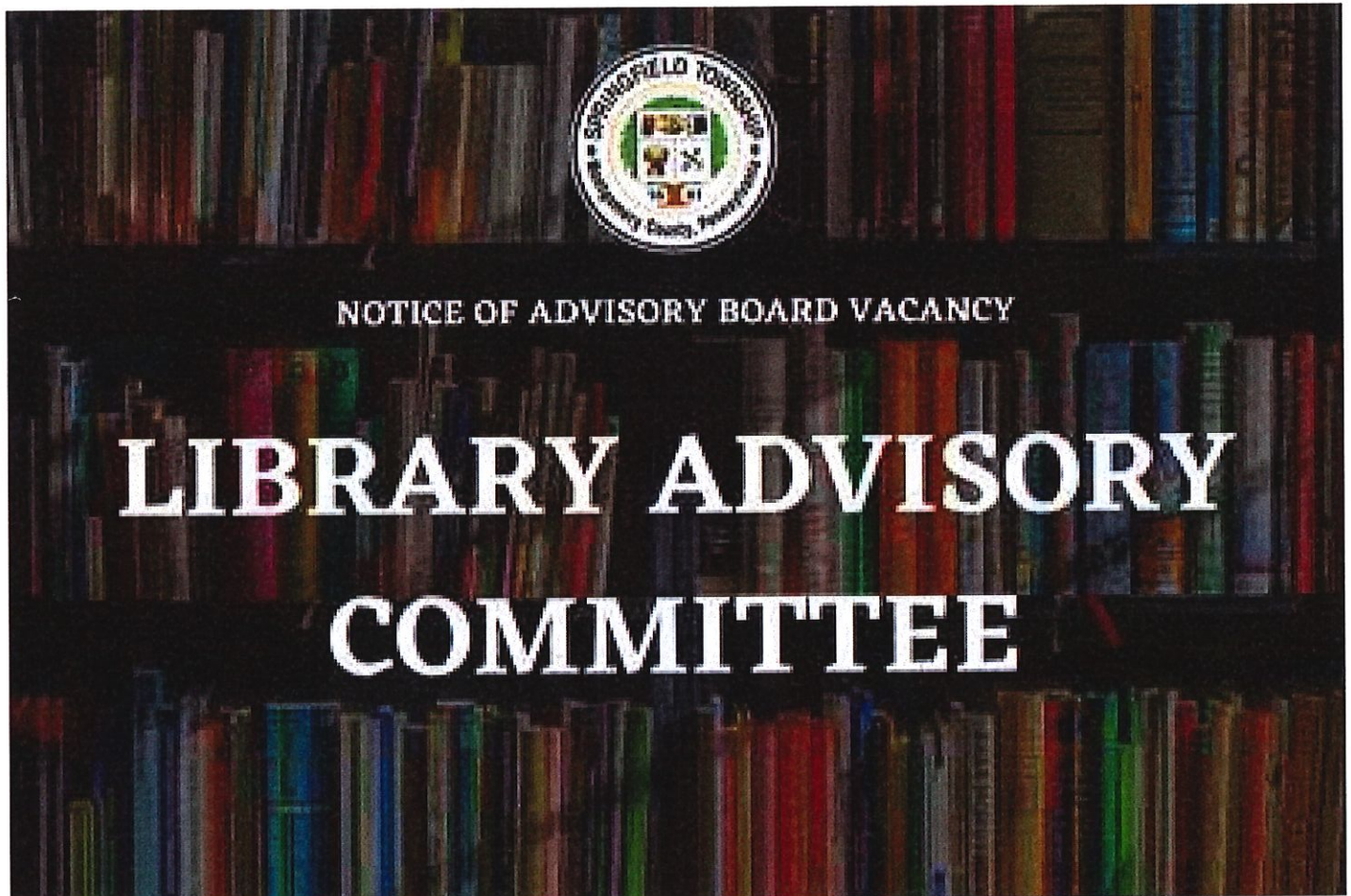
How to Apply

If you are interested in volunteering to serve on this advisory committee please send a letter of interest or resume to the attention of Mr. Michael Taylor, Township Manager, 1510 Paper Mill Road, Wyndmoor, PA 19038 or via email at mtaylor@springfieldmontco.org. For more information, please visit our website, www.springfieldmontco.org.

Want to receive Township news and alert notifications?

Be sure to subscribe to your favorite Township notification categories by registering on the Springfield Township website (www.springfieldmontco.org)! It only takes a few minutes and is incredibly easy. [Check out our video tutorial on how to sign up today.](#)

Recent Articles



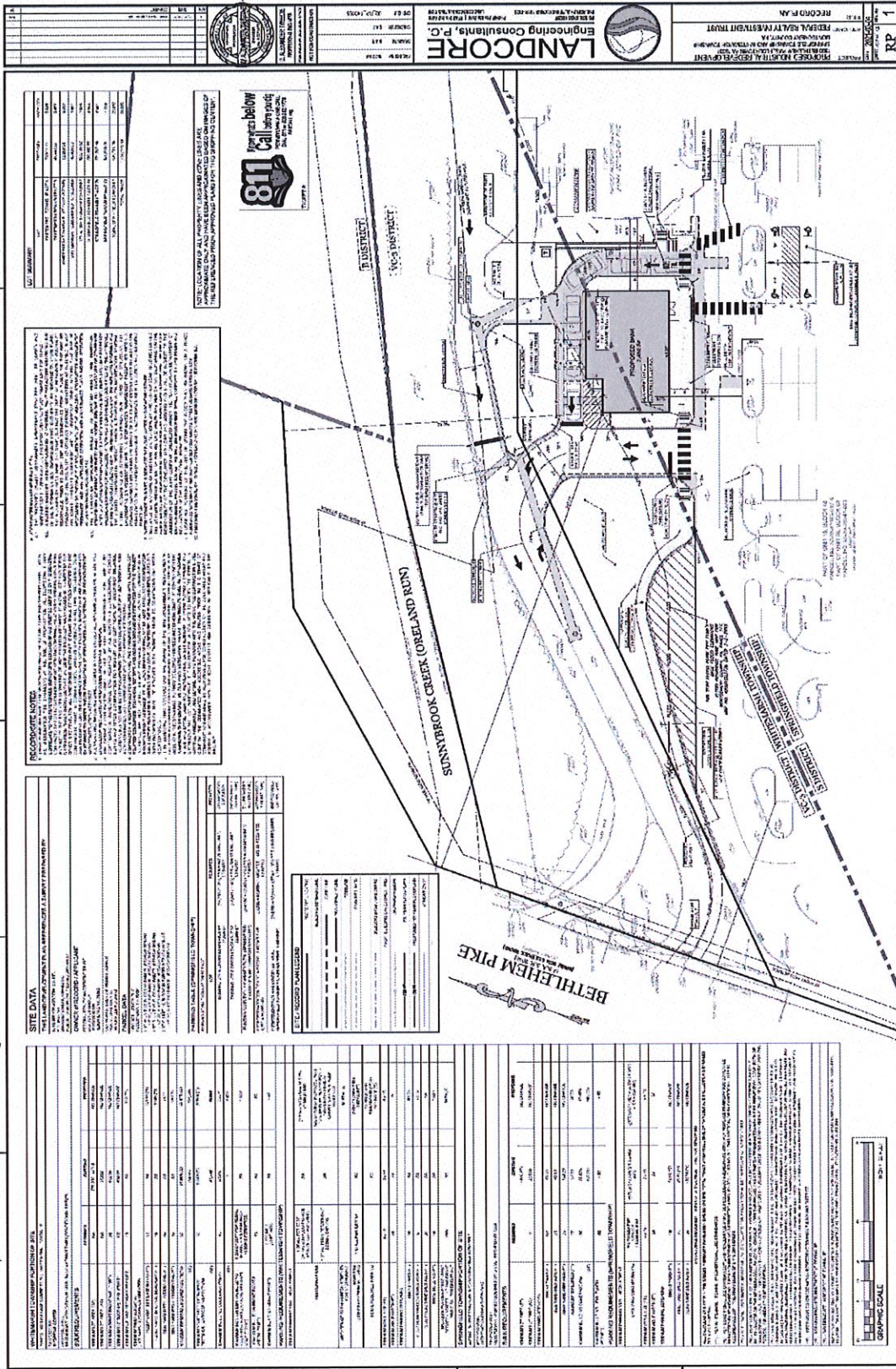


Flourtown Shopping Center
MCPC #200111004

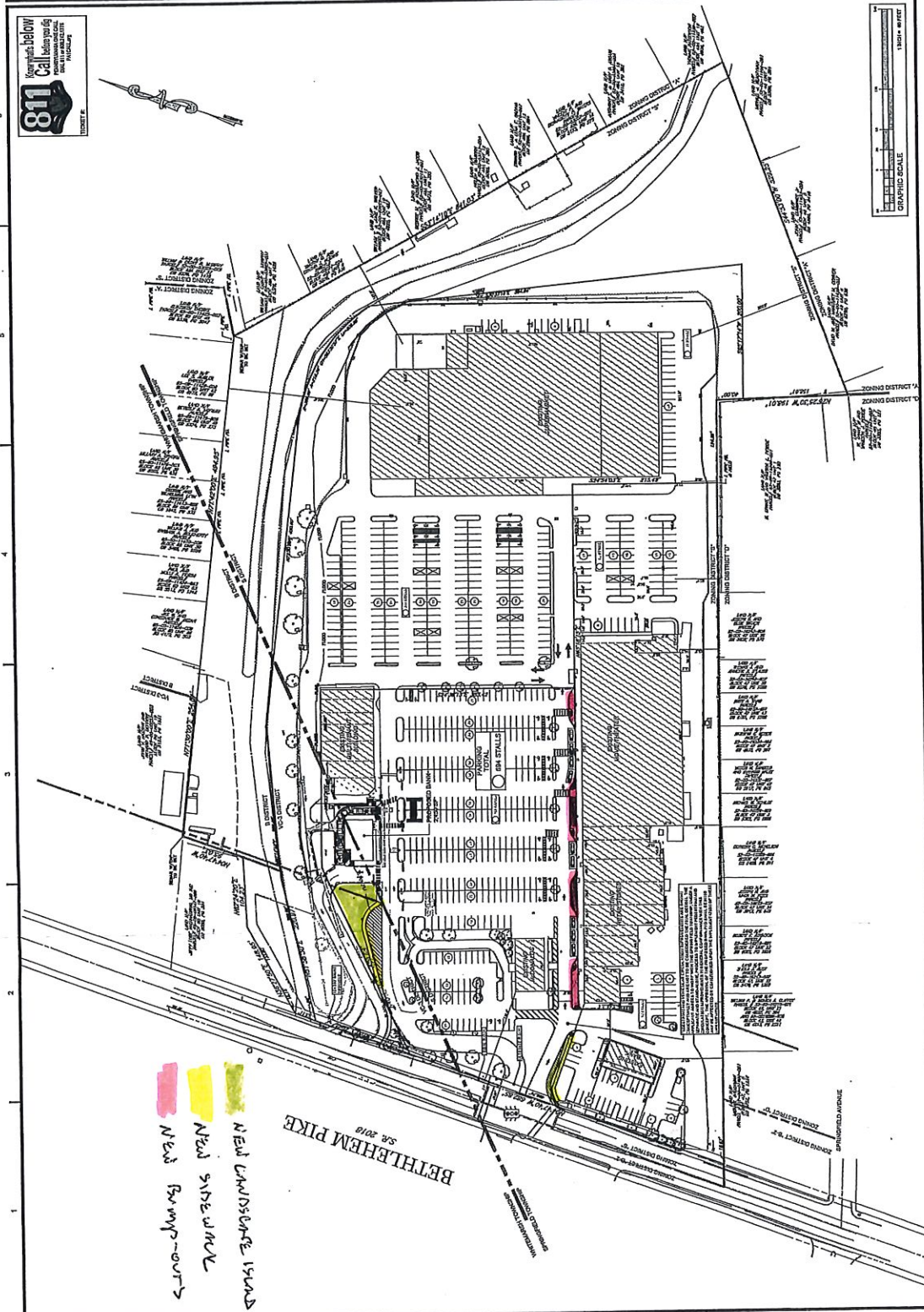
Montgomery
County
Planning
Commission
Montgomery County Courthouse - Planning Commission
100 State St. • Harrisburg, PA 17104-0221
(717) 261-3722 • (717) 261-3747
www.montgomery.org/planning
Aerial photography provided by ViewMap

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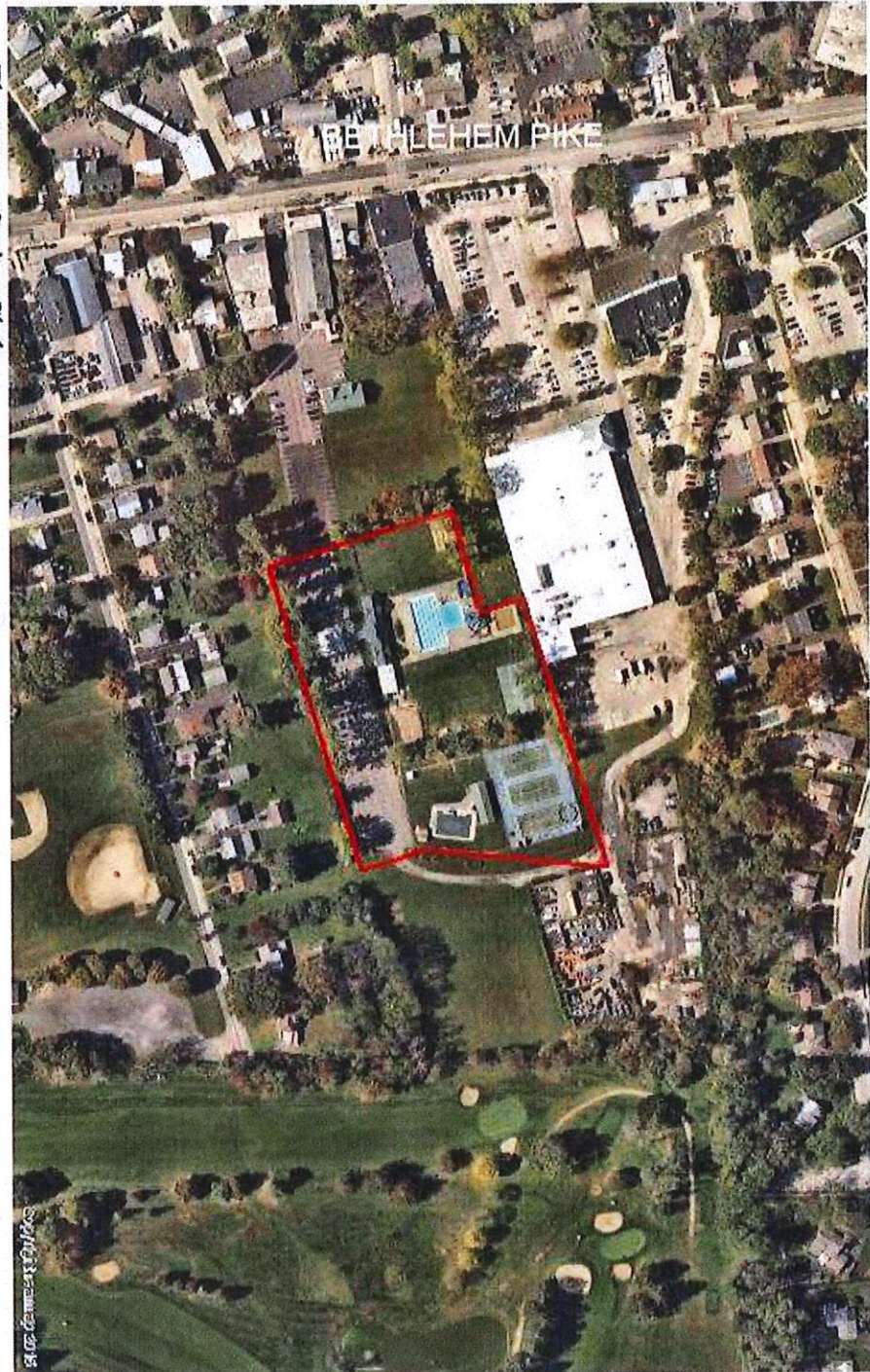


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NO. REV. DATE BY CHKD.	DATE: 08/20/2013 BY: [Signature] CHKD.: [Signature]	PROJECT: PROPOSED NEW RETAIL, RESIDENTIAL, COMMERCIAL DEVELOPMENT CLIENT: PERENAL REALTY INVESTMENT TRUST DATE: 08/20/2013 SCALE: 1" = 100'



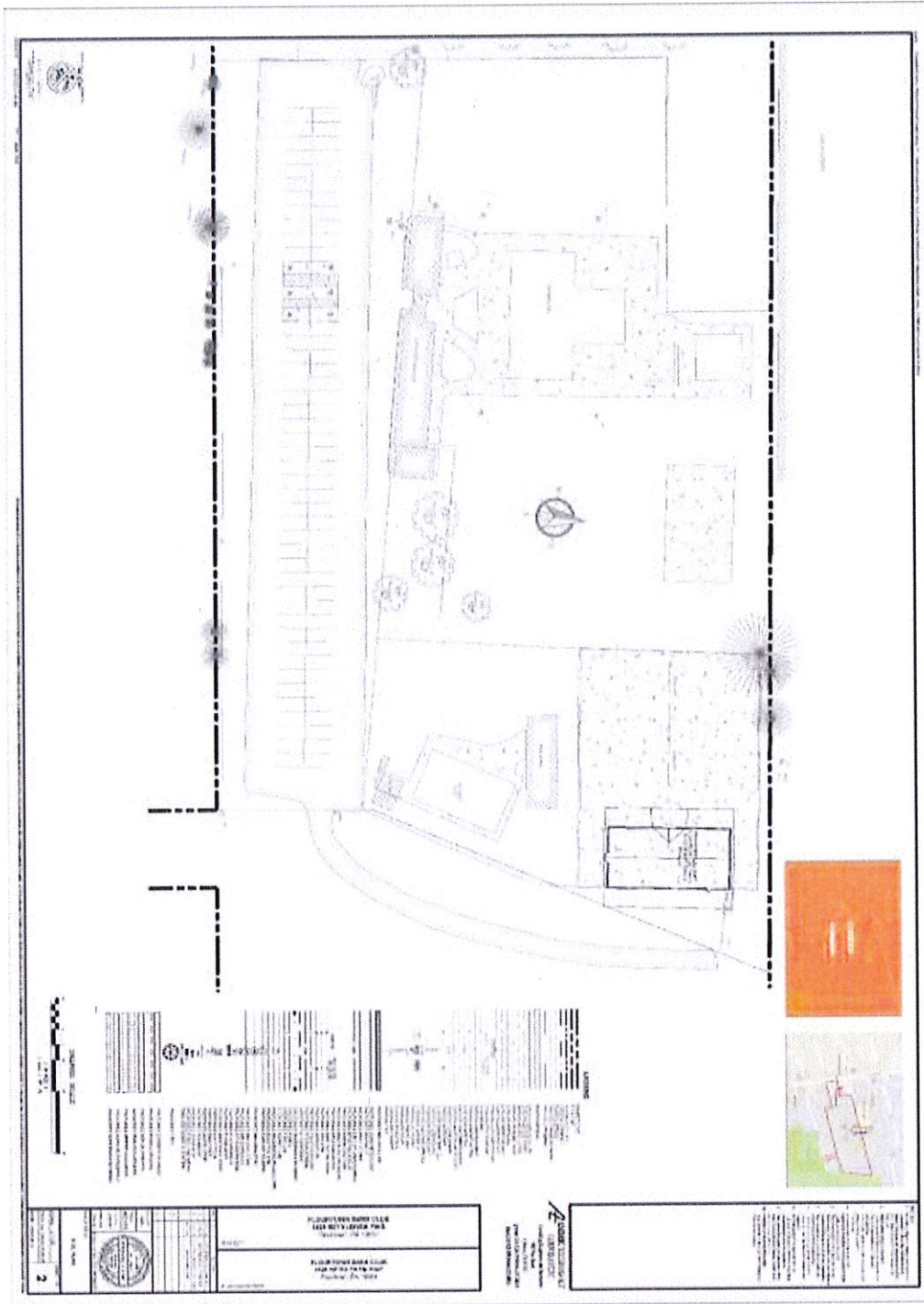
NEW SWAMP-OUTS
 NEW SIDEWALK
 NEW LANDSCAPE ISLANDS

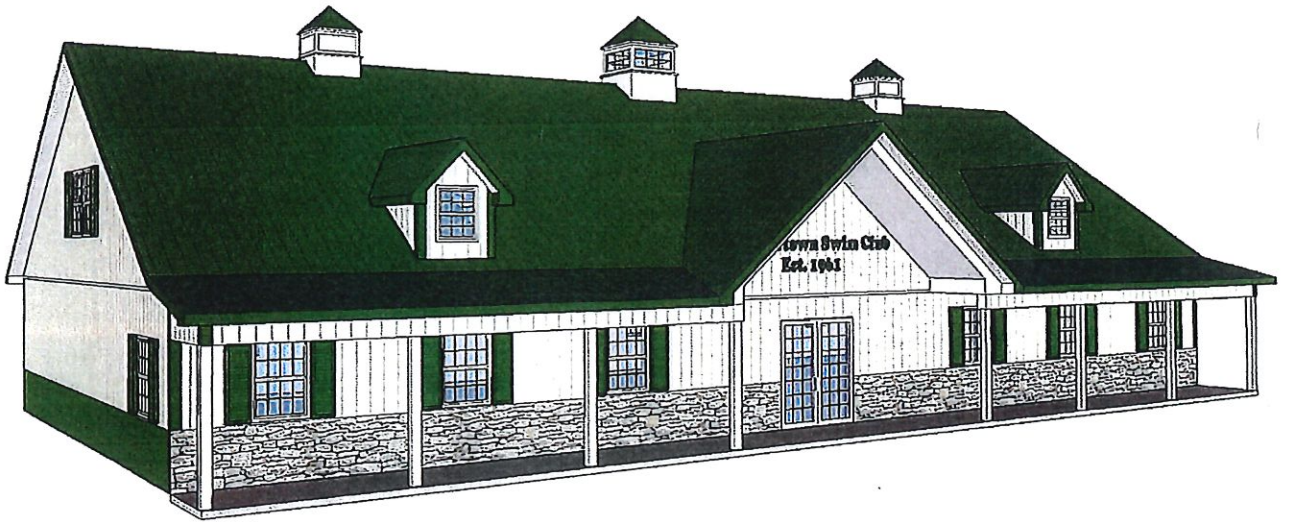
BETHEHEM PIKE
SR 2018



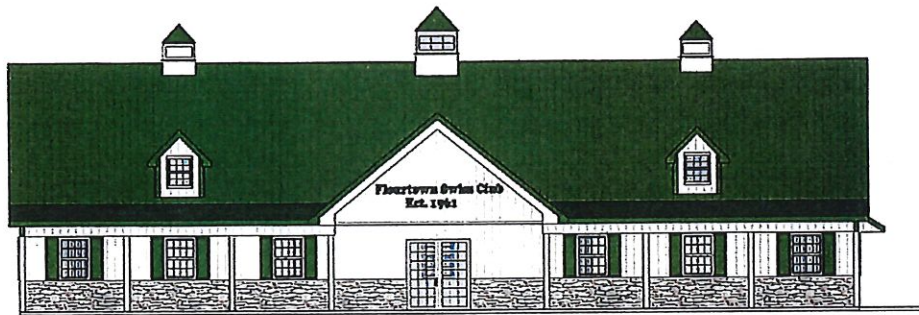
Flourtown Swim Club
MCPC #210321001

Montgomery County
Planning Commission
Montgomery County, Chairwoman - Planning Commission
1700 Locust St. • Philadelphia, PA 19103-2021
PA 19102-263722 • (610) 272-2141
www.montgomeryplanning.org
Aerial photography provided by Earthmap

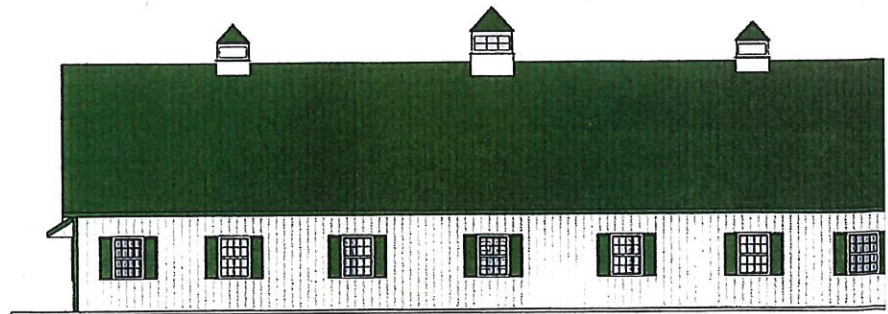




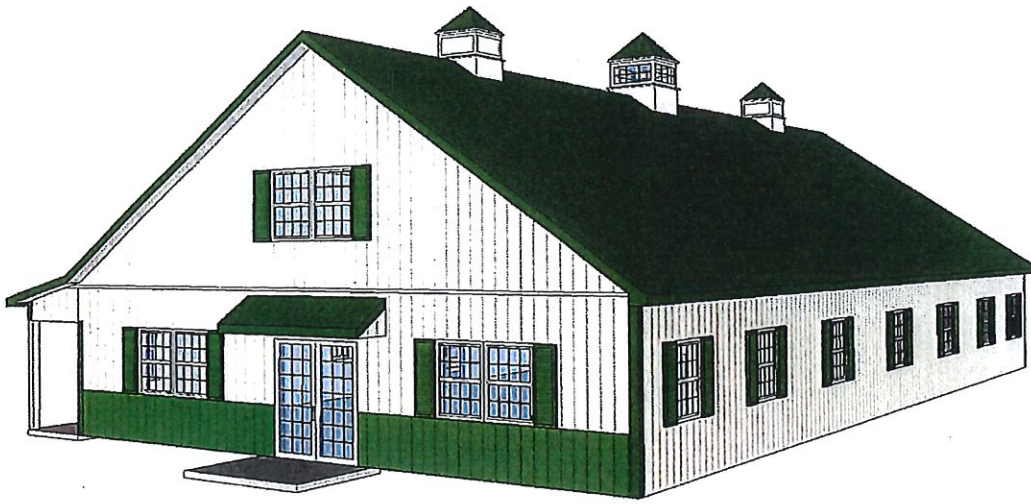
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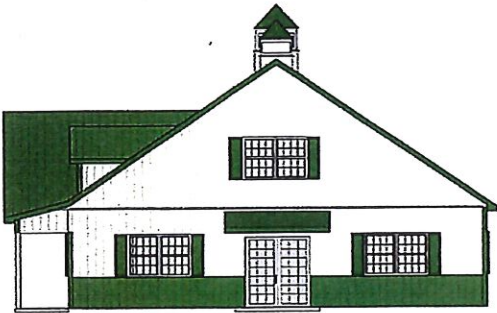
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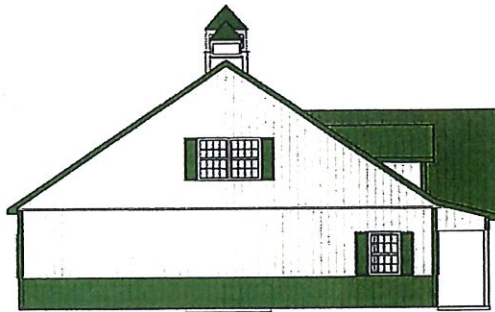
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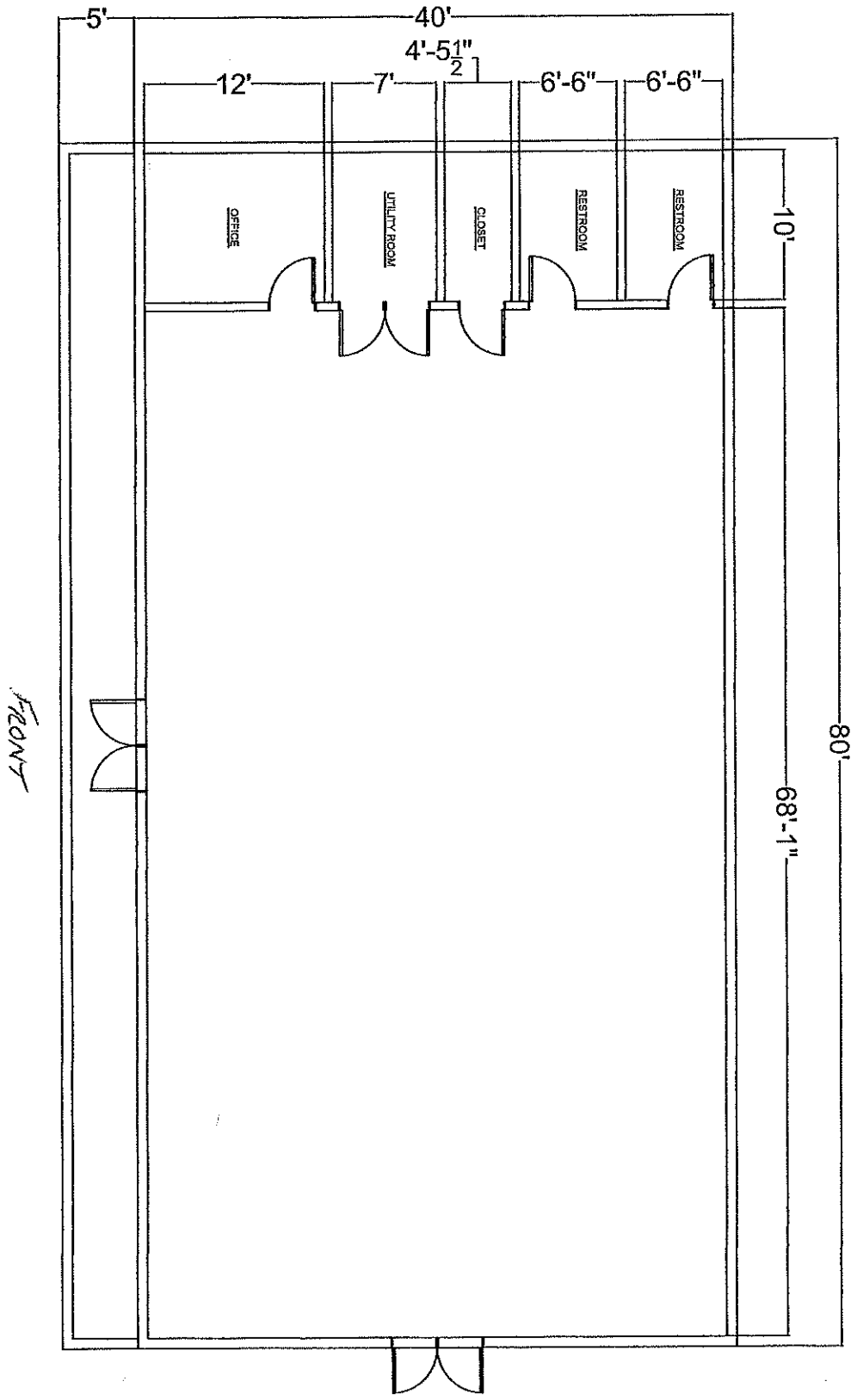


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6 LEFT

FLOOR PLAN



INTRODUCTION

SPRINGFIELD'S 2005 OPEN SPACE PLAN

GREEN FIELDS/GREEN TOWNS PROGRAM

In 2003, a referendum to fund open space and green infrastructure projects was passed in Montgomery County. This funding was distributed to municipalities, private non-profit conservation organizations and the county to preserve more open space and enhance the livability of existing communities throughout the County. A similar referendum was passed in 1993. At that time, Springfield drafted its 1996 Open Space and Environmental Resource Protection Plan, which has served as a valuable guide to the Township's open space activities for the past nine years.

Under the new program, Springfield is eligible to receive a total of \$1,397,301 for open space planning and implementation between April 2004 and April 2008. This grant requires matching funds equal to twenty percent of project costs from the township. The County grants come with several conditions. The most important condition is that any land purchased with grant money must be permanently preserved as open space or for active recreation. Another condition is that Springfield must complete and adopt an updated Open Space Plan. This plan must be approved by the County's Open Space Board before grant money can be disbursed.

DEVELOPING AN OPEN SPACE PLAN

In May 2004, the Springfield Open Space Committee was formed according to the requirements of the Green Fields/Green Towns Program. Members represented the Springfield Planning Commission, The Shade Tree Commission, the Park and Recreation Board, the Board of Commissioners, and several neighborhood representatives with

interest in and knowledge of open space issues. Liaisons from the Township as well as the Montgomery County Planning Commission also served on the committee. The Open Space Committee held public meetings on a monthly basis from June 2004 through May 2005 to develop this plan. The committee presented a draft version of

this plan to the Board of Commissioners during a regularly scheduled meeting, as well as to the community during a public hearing. Comments were solicited from the public and incorporated into the final document.

The 2005 Open Space Plan was reviewed and approved by the County Open Space Board prior to adoption by the Township. This assures that the plan's recommendations are eligible for funding through the Green Fields/Green Towns program (this does not, however, guarantee that any specific project will receive funding). In addition to County Open Space grants, grants from other agencies will be sought to implement many of the recommendations in this plan. Upon completion of this plan, Springfield Township will embark on implementing the recommendations listed in Chapter 10 by writing specific project proposals and applying to various organizations and agencies for grants.

The Open Space Committee made every effort to consider all aspects of open space planning relevant to the Township. However, in the event that a project, program or policy was overlooked, the plan may be amended by following the procedure outlined in section 302 (a) of the Pennsylvania Municipalities Planning Code.

PURPOSE OF THIS PLAN

This plan was developed for two major purposes. First, as previously stated, it is a requirement that must be filled in order to be eligible to apply for funding through the Green Fields/Green Towns program. It was also developed to serve as a guide to the Township in acquiring new open space, forging connections through existing open space via an enhanced trail network, and developing programs and policies to preserve and maintain active and passive open space in the Township.

THE OLD PLAN VS. THE NEW PLAN

Springfield's previous open space was vital in guiding the Township's open space development. That plan mainly emphasized land acquisition and trail development. As a result of the plan, several projects were implemented, including the acquisition of The Black Horse Inn and Laurel Beech

Park, the development of the AAA zoning district, acquisition of a trail easement on Northwest Avenue, utilization of a \$48,997 tree-planting grant, and establishment of a private conservation easement on the Wharton-Sinkler Tract.

In addition to acquisition and trail development, the new plan emphasizes revitalization of Green Infrastructure such as park improvements, enhanced tree-planting activities, and gateway installation. It also focuses on Heritage Resource Conservation, County Trail Connections, and Floodplain Restoration. These items were not previously eligible for funding through County Open Space grants, and are a new feature of the 2005 Green Fields/Green Towns Program.

RECOMMENDATIONS

The Open Space Committee has included over eighty recommendations for open space projects, programs and policies. Each recommendation is described in detail in Chapter 10. It is also listed along with its priority level, the party responsible for implementation, and potential funding sources in the implementation matrix in Chapter 11.

WORKING WITH OUR NEIGHBORS

Springfield Township has been working with its neighbors, including Whitemarsh Township, Upper Dublin Township, Cheltenham Township, and the City of Philadelphia, to develop this open space plan. It will continue to work with them to implement the recommendations of this plan and to work on planning efforts in the future.

RESOLUTION NO. 1091

WHEREAS, the Montgomery County Open Space Preservation Program requires the Board of Commissioners of Springfield Township to establish a committee to prepare and update the local Open Space plan; and

WHEREAS, the Board of Commissioners of Springfield Township is responsible for directing the preparation of the plan in accordance with the requirements and standards of the Montgomery County Open Space Preservation Program; and

WHEREAS, the committee is provided with opportunities for active participation in the actual plan preparation process, and

WHEREAS, the Committee shall be comprised of one member of the Board of Commissioners of Springfield Township; one member of the Springfield Township Planning Commission; one member of a local revitalization committee; one member of the Springfield Township Park and Recreation Advisory Committee or Shade Tree Commission, the Director of Parks and Recreation for Springfield Township, two residents of Springfield Township; a Staff member of the Montgomery County Planning Commission to serve as a non-voting member of the committee, and a Township Staff member to serve as a non-voting member of the committee.

NOW, THEREFORE, BE IT RESOLVED: That the Board of Commissioners of Springfield Township does hereby authorize the formation of the Springfield Township Open Space Planning Committee for the purpose of preparing and coordinating the Springfield Township Open Space Plan.

ADOPTED by unanimous vote this 14th day of July, 2004.

BOARD OF COMMISSIONERS OF
SPRINGFIELD TOWNSHIP

By: _____
Glenn A. Schaum, President

Attest:  _____
Donald E. Berger, Jr., Secretary

BY-LAWS
SPRINGFIELD TOWNSHIP OPEN SPACE PLANNING COMMITTEE

ARTICLE I
Name of Organization

- 1.1 The name of this organization shall be the Springfield Township Open Space Planning Committee

ARTICLE II
Authorization

- 2.1 Authorization for the establishment of this Committee is by Resolution No. 1091 of the Springfield Township Board of Commissioners dated July 14, 2004.

ARTICLE III
Membership

- 3.1 The committee shall be comprised of seven (7) members; one member of the Board of Commissioners of Springfield Township, one member of the Springfield Township Planning Commission; one member of the Springfield Township Park and Recreation Advisory Committee or Shade Tree Commission, one member of a local revitalization Committee; the Park and Recreation Director; two residents of Springfield Township; a staff member of the Montgomery County Planning Commission to serve as a non-voting member of the Committee; and a Township Staff member to serve as a non-voting member of the Committee.
- 3.2 With the exception of the first appointment, the terms of service on the Open Space Planning Committee shall be for a term of three (3) years as approved by the Township Board of Commissioners. The first term appointments to the Committee shall be staggered in the following manner: Two (2) appointments one (1) year; Two (2) appointments two (2) years; three (3) appointments three (3) years.

ARTICLE IV
Officers

- 4.1 The officers of the Open Space Committee shall consist of a Chairman, Vice-Chairman, and a Secretary. The Chairman and Vice-Chairman shall be members of the committee but the secretary need not be a member of the Committee.
- 4.2 The Chairman shall preside at all meetings and hearings of the Committee and shall have the duties normally conferred by parliamentary usage on such

officers. At each meeting, the Chairman shall submit such recommendations and information as considered proper concerning the business affairs and policies of the committee.

- 4.3 The Vice-Chairman shall act for the Chairman in the absence or incapacity of the Chairman. In case of the resignation or death of the Chairman, the Vice-Chairman shall perform such duties as are imposed on the Chairman until such time as the new Chairman shall be appointed.
- 4.4 The Secretary shall keep the minutes and records of the Committee, and with the assistance of such Township Staff as is available, shall prepare the agenda of regular and special meetings under the direction of the chairman, provide notice of all meetings to Committee members, arrange proper and legal notice of hearings, attend to correspondence of the Committee, and such other duties as are normally carried out by a Secretary.
- 4.5 The officers of the Committee shall perform such other duties and functions as may from time to time be required by the Committee or by the by-laws and rules and regulations of the Committee.

ARTICLE V Meetings

- 5.1 Meetings shall be held at the Springfield Township Building located at 1510 Paper Mill Road, Wyndmoor, Pennsylvania, at a time and place determined by the committee. Notice of the public meeting shall be posted on a prominent bulletin board in the Township Building and Library.
- 5.2 A quorum shall consist of four (4) members of the Committee. The number of votes necessary to transact business shall be by a simple majority of those present. Voting shall be by roll call, or aye or nay. A record of the roll call shall be kept as a part of the minutes.
- 5.3 Special meetings may be called by the Chairman. It shall be the duty of the Chairman to call a special meeting when requested to do so by three members of the Committee, other than the Chairman. All members of the committee must be notified in writing not less than three days in advance of such special meetings. In extreme emergencies, notification can be by telephone.
- 5.4 All meetings or portions of meetings at which official action is taken shall be open to the general public.
- 5.5 The Chairman may appoint committees for specific purposes as necessary.

- 5.6 Unless otherwise specified, Robert's Rules of Order modified shall govern the proceedings at the meetings of this Committee.

ARTICLE VI
Employees

- 6.1 Within the limits imposed by the funds available under the budget for its use, the Committee may recommend the employ of consultants to the Board of Commissioners as necessary to aid in its work.

ARTICLE VII
Hearings

- 7.1 The Committee may hold public hearings approved by the Board of Commissioners when it decides that such hearings will be in the public interest. The Committee shall hold public hearings when it is required by law.
- 7.2 Notice of the time and place of such a hearing, when on matters of widespread interest, shall be published once a week for two successive weeks in at least one newspaper of general circulation in the township not earlier than three weeks prior to the hearing. For matters of limited territorial interest, notice shall be given in such a manner as is deemed appropriate by the Committee.
- 7.3 The matter before the Committee shall be presented in summary and parties in interest shall have the privilege of the floor.

ARTICLE VIII
Amendments

- 8.1 The Committee may recommend to the Board of Commissioners that these bylaws be amended with an affirmative vote of at least four (4) members of the Committee at a regular or special meeting.
- 8.2 Notice of the proposed change in the bylaws must be given to all committee members at least 30 days prior to the meeting at which the amendments to the bylaws shall be considered and this notice must be in writing by first class mail to all members of the committee.

ADOPTED by the Board of Commissioners of Springfield Township, on July 14, 2004.

POLICY STATEMENT OF THE
SPRINGFIELD TOWNSHIP OPEN SPACE PLANNING COMMITTEE
ON ETHICS, CONFLICT OF INTEREST AND DISCLOSURE

CODE OF CONDUCT

1. It shall be the duty of each member of the Open space Planning Committee to disclose on the public records of the Committee any actual or apparent conflict of interest between that member and any item of business before the Committee. Said disclosure shall be noted in writing in the official minutes of a Committee meeting, or in a separate letter attached to those minutes.
2. Any Committee member experiencing a conflict of interest shall declare his or her interest publicly, abstain from voting on the matter, keep out of deliberations on the matter, and leave any chamber in which such deliberations are to take place. That member shall not discuss the matter privately with the staff or with any other Committee member.
3. Any Committee member to whom some pecuniary benefit may come as the result of some public action by the Springfield Township Open Space Planning Committee shall not be a participant in that action.
4. No Committee member shall engage in any transaction with the Open Space Planning Committee or the staff in which he or she has a financial interest, direct or indirect, unless the transaction is fully disclosed.
5. Each Committee member shall comply in all respects with the applicable provisions of the Commonwealth of Pennsylvania Public Officials and Employees Ethics Law.

PROCEDURE

This Code is adopted as a voluntary act on the part of the seven (7) members constituting the Springfield Township Open Space Planning Committee. It is a self-enforcing and self-determining code on the part of each member and collectively as a Committee. When a Committee member has doubt as to the potential applicability of any provisions of this code to a particular situation, he or she should apply to the fellow committee members for any advisory opinion.

EFFECTIVE DATE

This code shall take effect immediately upon acceptance by vote of the Committee and shall be applicable from that date forward.

**RECYCLING REPORT
STATISTICAL DATA
FOR THE MONTH OF JANUARY 2022**

	<u>THIS MONTH</u>	<u>YEAR TO DATE</u>	<u>2022 BUDGET</u>
Materials Collected (tons)			
Single Stream Recyclables	181.9	181.9	2,231
Householder Participation			
No. of Curb Stops	19,530	19,530	281,276
Percent of Total (7,200)	67.8	67.8	75.0
Avg. Lbs. per Curb Stop	18.6	18.6	15.9
Sales Value of Recyclables (net)			
Single Stream Recyclables (\$31.18)*	(5,671.64)	(5,671.64)	(101,323.33)
Disposal Savings/Cost Avoidance			
Authority Tipping Fee @ \$62.03	11,283.26	11,283.26	138,407.54
State Performance Grant			
Prorated Annual Award	2,470.42	2,470.42	29,645.00
Grand Total Sales/Savings	8,082.04	8,082.04	66,729.21
Cost of Collection (prorated)			
Labor and Overhead	(25,313.00)	(25,313.00)	(303,756.00)
Equipment Cost	(3,816.75)	(3,816.75)	(45,801.00)
General Expense/Recycling Center	(83.33)	(83.33)	(1,000.00)
Total Cost	(29,213.08)	(29,213.08)	(350,557.00)
Net Income and Saving	(21,131.04)	(21,131.04)	(283,827.79)

*Disposal Rate Adjusts Monthly



SPRINGFIELD TOWNSHIP
ENVIRONMENTAL ADVISORY
COMMISSION

January 31, 2022

Eddie Graham, Esq., President
Board of Commissioners
Springfield Township, Montgomery County
1510 Paper Mill Road
Wyndmoor, PA 19038

Subject: Request to Adopt Energy Transition Transportation Plan

Dear President Graham:

In January 2019, the Springfield Board of Commissioners unanimously passed Resolution 1469 to make Springfield a clean energy municipality, eliminating our dependence on fossil fuels. This will help slow climate change while increasing social equity, since fossil fuel pollution disproportionately hurts people of color and those in poverty. The commissioners committed that by 2035, our municipal buildings would use 100% renewable electricity, and by 2050, our fleet would be 100% clean, as would the building heat in our buildings. We have branded this program *Renewable Springfield*.

The EAC has been working steadily to provide guidance to the commissioners in meeting these goals. The township had an energy audit of our buildings performed in late 2019, which, once normal times return, should result in ongoing savings of \$16,000 per year in electricity costs, and the avoidance of 200,000 pounds of carbon pollution per year. The Board's commitment to buy hybrid police cruisers moving forward is already resulting in savings. Although we don't have a full year of data yet, here's some good news: We compared the gas consumption in December of one of our hybrids to one of the traditional police cruisers. Each drove basically the same number of miles, but the hybrid used less than half the gas. Again, we're saving taxpayer dollars while cutting pollution.

EAC member Desiree Rammon presented to you last year the draft Transportation Plan that she has written, which can help guide your decisions in coming years to reach our clean energy goals. The EAC voted in January to accept the draft Transportation Plan as final.

The EAC now asks the Commissioners to formally adopt the EAC Energy Transition Transportation Plan. Doing so, and featuring it on our website and publications, will remind residents of the township's commitments under *Renewable Springfield*, and reinforce the leadership of the Commissioners to achieve the goals of the program. (Note that we expect to update the plan at least annually, as technology and market conditions change and offer us more and better choices).

This is the first completed section of our Energy Transition Plan. We are working on the Municipal Buildings section of the plan, which will lay out the path to clean electricity and heat. We look forward to presenting you that draft plan in the months ahead. We're also working to educate our residents about how they too can reach these objectives.

The EAC is most appreciative of the universal support we've received from the township: from the Commissioners, as well as our senior staff (specifically, Mike Taylor, Brandon Ford, and Ian Hammer). Springfield continues to be seen as the leader on these efforts. Both staff and EAC members are regularly contacted by municipalities in our area to inquire about what we've already done, and to find out what we're working on. We are the go-to thought leaders.

Sincerely,

Joy Bergey (signed electronically)
EAC Vice-Chair
Springfield Township

DRAFT

Springfield Township Energy Transition Plan: Clean Transportation By 2050

Executive Summary

In January 2019, Springfield Township adopted a Renewable Energy Resolution committing to a clean energy transition goal for electricity by 2035 and for all municipal transportation and building heat to operate on clean energy by 2050. The effort to achieve the goal of clean transportation by 2050 involves a continuing conscious emphasis on energy usage and efficiency, not only with municipal sources of carbon emissions, but also among the larger community. This section of the Township's Energy Transition Plan discusses and provides recommendations for a transitioning of the municipal fleet, currently energized by fossil fuels, to vehicles and equipment that obtains renewable energy from the electric grid.

The greatest effect in reducing carbon emissions from municipal sources lies in electrifying all currently fossil fuel-fired vehicles and equipment owned and operated by the township. The benefits of electric vehicles are wide-ranging and well-documented. An electric car is a car which is propelled by one or more electric motors, using energy stored in rechargeable batteries. These vehicles can have significantly lower fuel costs and total costs of ownership compared with gasoline-powered vehicles. Importantly, they are one of the most promising ways of reducing carbon emissions from the transportation sector, which is the largest contributor to U.S. greenhouse gas emissions.

The municipal fleet of Springfield Township in Montgomery County includes a variety of fossil fuel-powered vehicles and equipment, from police patrol cars to garbage trucks to lawn mowers. Based on fuel usage in 2019, the township fleet emitted nearly 600 metric tons CO₂ equivalent (MTeCO₂). Mobile sources comprised the largest combined municipal source, approximating over 60% of total emissions. The path toward ZERO municipal mobile emissions in Springfield Township has been initiated with the replacement of police patrol cars with hybrid (gas/electric) vehicles, followed by retirement of these hybrid cars as administrative vehicles. These hybrid vehicles, in addition to all vehicles and equipment utilized by Public Works with the exception of refuse, will be replaced by all-electric vehicles upon achieving the municipal goal of clean electricity in 2035. Refuse vehicles will be replaced by all-electric vehicles starting in 2040. This is a non-binding, staggered approach that can be altered as needed by the township.

Research on electric vehicles and equipment, including available technology and financial assistance, during the municipal procurement process is central to the inclusion of hybrid and electric-only vehicles and equipment in the township fleet. In addition, we recommend that Springfield Township take steps to encourage an increase of private EV purchase, such as promoting outreach and creating policies, to enable community-wide adoption of EVs. In developing policy, there should be an overarching goal of making Springfield communities more walkable and bikeable, therefore, more sustainable and healthy. A walkable neighborhood is one in which residents are able to accomplish many of their daily activities without a car.

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In November 2020, the Township Board of Commissioners made the decision to buy two hybrid police patrol vehicles for 2021, thus initiating the path to a clean municipal fleet. The first of these hybrid vehicles was patrolling the streets of Springfield in June 2021. The transition has begun!



I. Introduction

The benefits of electric vehicles (EVs) are wide-ranging and well-documented. An **electric car** is a car which is propelled by one or more electric motors, using energy stored in rechargeable batteries. Compared to internal combustion engine (ICE) cars, electric cars are quieter, have no exhaust emissions and lower emissions overall. In the United States, as of 2020, the total cost of ownership of newer EVs is cheaper than that of comparable ICE cars, due to lower fueling and maintenance costs (*Consumer Reports, November 2020*). Charging an electric car can easily be done at a variety of charging stations; these charging stations can be installed at both houses and in public areas.

While an electric car's power source is not explicitly an on-board battery, electric cars with motors powered by other energy sources are typically referred to by a different name. For example, an electric car using solar panels as a power source is a solar car, and an electric car powered by a gasoline generator is a form of hybrid car. Thus, an electric car that derives its power from an on-board battery pack is a form of electric-only or battery electric vehicle (BEV). For the sake of clarity, the basic types of electric vehicles are defined in the table below.

<p style="text-align: center;"><i>Electric Vehicle (EV)</i></p> <p>Overall term for vehicles that derive all or part of their power from electricity supplied by the electric grid. They include AEVs, HEVs and PHEVs.</p>
<p style="text-align: center;"><i>All-Electric Vehicle (AEV)</i></p> <p>Vehicles powered by one or more electric motors. They receive electricity by plugging into the grid and store it in batteries. They consume no petroleum-based fuel and produce no tailpipe emissions. AEVs include <i>Battery Electric Vehicles (BEVs)</i> and <i>Fuel Cell Electric Vehicles (FCEVs)</i>.</p>
<p style="text-align: center;"><i>Plug-in Hybrid Electric Vehicle (PHEV)</i></p> <p>Vehicles that use batteries to power an electric motor, plug into the electric grid to charge, and use a petroleum-based or alternative fuel to power the internal combustion engine. Some types of PHEVs are also called <i>extended-range electric vehicles (EREVs)</i>.</p>
<p style="text-align: center;"><i>Hybrid Electric Vehicle (HEV)</i></p> <p>Low-emission vehicles that use an electric motor to assist gas-powered engines. All energy comes from gasoline.</p>

The reduction in tailpipe emissions from EVs ties in to public health benefits associated with better air quality. Furthermore, they reduce US dependence on foreign oil, which is beneficial for national security. As noted above, EVs can have significantly lower fuel costs and total cost of ownership (TCO) compared with gasoline-powered vehicles. Importantly, they are one of the most promising ways of reducing carbon emissions from the transportation sector, which is the largest contributor to US greenhouse gas (GHG) emissions.

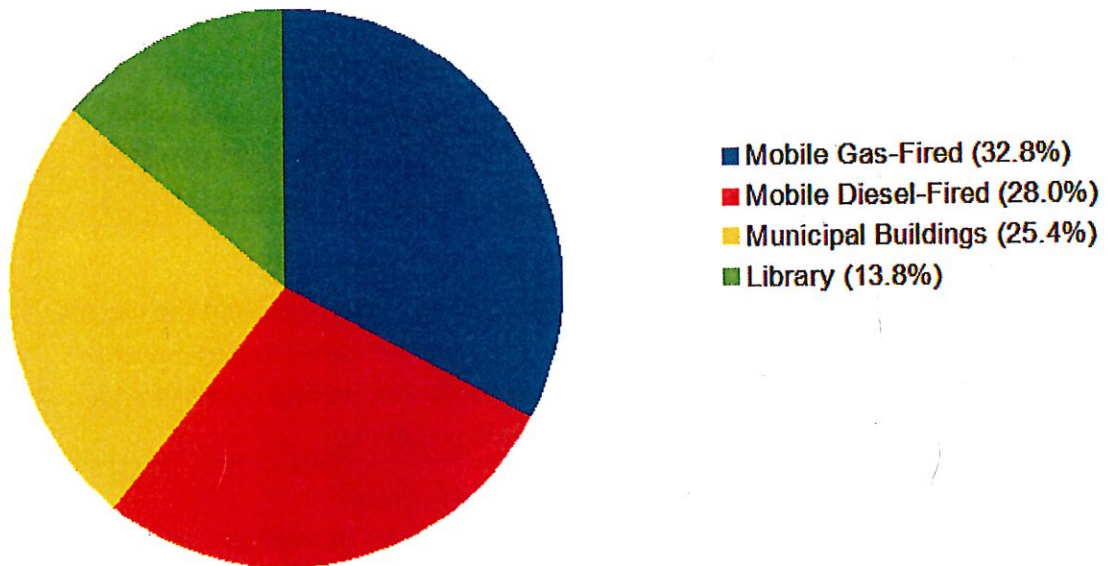
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In the following sections, we will provide background on Springfield Township’s vehicle fleet inventory and fuel usage, using 2019 as a baseline, and discuss strategies to encourage the use of clean transportation by residents and business owners within township boundaries. These community strategies emphasize improved walkability and bikeability in the township to promote cleaner and healthier living. At the heart of our discussion, however, is a proposed pathway to ownership of a 100% clean municipal fleet by 2050.

II. Township Fleet

The municipal fleet of Springfield Township in Montgomery County includes a variety of fossil fuel-powered vehicles and equipment, from police patrol cars to garbage trucks to lawn mowers. In 2019, Springfield Township used approximately 39,652 gallons of gasoline and 26,788 gallons of diesel fuel, spending approximately \$125,000 on fuel to power its fleet of vehicles. Based on fuel usage, the township fleet emitted nearly 600 metric tons CO₂ equivalent (MTeCO₂). As can be seen from the chart of municipal carbon emissions shown below, mobile sources comprised the largest combined municipal source, approximating over 60% of total emissions.

MUNICIPAL CARBON EMISSIONS BY SOURCE
Springfield Township, Montgomery County



It should be noted that the fuel usage and carbon emissions provided above include emergency vehicles, such as fire trucks and ambulances, that serve the community. These emergency vehicles may refill from fuel storage provided by the Township; however, the fire and EMS departments are self-standing entities not operated or managed by the Township. Although the emergency departments repay the Township for fuel used, emergency vehicle emissions are not included in the emissions reduction strategy included later in this section.

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Replacement vehicles and equipment for Springfield Township are purchased on a staggered schedule. The procurement process for the Township involves use of **COSTARS**, the Commonwealth of Pennsylvania's cooperative purchasing program. This program serves as a conduit through which registered and eligible local public procurement units (LPPUs) and state-affiliated entities (together "Members") are able to leverage contracts established by the Pennsylvania Department of General Services (DGS) to effectively and efficiently identify suppliers with whom to do business. In the case of police and administrative vehicles, two patrol vehicles are replaced on an annual basis, and retired patrol vehicles are then used as administrative vehicles. The Township utilizes a variety of models for the police fleet, including the Ford Interceptor. In November 2020, the Township Board of Commissioners made the decision to buy two hybrid police patrol vehicles for 2021, thus initiating the path to a clean municipal fleet.

A municipal inventory of gas- and diesel-powered mobile sources is provided below:

Department	Vehicle	Number of Gas-Powered	Number of Diesel-Powered
Police	Police Cruisers	14	
Administrative	Administrative Vehicles	5	
Public Works	Pickup	9	
	Small Highway Dump	4	
	Bucket Truck	1	
	Riding Mowers	5	
	Small Snow Equipment	5	
	Portable Generators	10	
	Chain Saws	8	
	Backpack Blowers	4	
	Refuse & Recycle Trucks		7
	Large Highway Dump		4
	Winter Maintenance Truck		4
	Sweeper		1
	Sewer / Flush Truck		1
	Compressors		2
	Tub Grinders		1
	Leaf Machine		1
Tractor		1	
Loader		3	
Building Generator		4	
TOTAL		65	29

III. Electric Vehicle Charging Opportunities

Currently, most EVs charge at home or work. Two types of EV charging meet national Society of Automotive Engineers (SAE) standards:

Level 1 charging uses a standard, grounded, three-prong, 120-volt outlet with a ground fault circuit interrupter and also requires a 15- to 20 amp fuse or circuit breaker. The typical charge time for 40 miles of electric driving is 8 to 10 hours at Level 1. BEVs with larger batteries and extended electric driving range may take as much as 12 to 21 hours to fully charge at Level 1.

Level 2 charging requires the installation of a 240-volt charging station (also known as electric vehicle supply equipment or EVSE) on a dedicated 40-amp circuit. Level 2 charging will typically charge an EV in about half the time of Level 1 and uses about the same amount of electricity as a central air conditioning unit when charging. This type of station may require upgrades to the home or building's electrical service.

Manufacturers also are working on fast-charging technologies that can recharge certain types of EVs in 30 minutes or less. **DC Fast Charging** (100 miles / hour of charging) is suited for drivers with no at home charging option (ex: those with no off-street parking availability) and for those on long trips who need to fill up without waiting a long time.

The various types of standard charging technology are shown in the graphic below.

	Level 1 (120V)	Level 2 (208/240V)	DC Fast Charging
Typical Duration of Charge Event	6-10 hours	1-3 hours	30 minutes
Range per hour of charging	5 miles	10-20 miles	75+ miles
Typical Users/Uses	<ul style="list-style-type: none"> ▪ Home use ▪ Employee parking during the work day ▪ Long term (8+ hours) parking at a commuter lot, charging while on a carpool, walking or transit trip 	<ul style="list-style-type: none"> ▪ Home use for EV owners wanting a faster charge ▪ Charging in a commercial area while shopping or doing business ▪ Parking at a commuter lot and charging while on a carpool, walking or transit trip 	<ul style="list-style-type: none"> ▪ Fast charging while on a long trip in order to reach a destination or extend the length of a trip
Desirable Characteristics	<ul style="list-style-type: none"> ▪ Workplaces ▪ Lit, safe area ▪ Pedestrian and transit connections 	<ul style="list-style-type: none"> ▪ Shopping, dining, restrooms etc. within walking distance ▪ Transit service ▪ Pedestrian facilities ▪ Lit, safe area 	<ul style="list-style-type: none"> ▪ Amenities at the charging site (food, coffee, Wi-Fi) ▪ Lit, safe area

Details regarding electric vehicle charging equipment may be found in the [*Electric Vehicle Charging Station Guidebook*](#), produced by the Vermont Energy Investment Corporation.

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For commercial EVSE installations, the township ordinance should clarify zoning requirements, allowing for such installations except in extreme circumstances where such installations would create hardships on property owners. Community or Design Guidelines should also be reviewed where they would arbitrarily create unnecessary barriers to EVSE installations, and where appropriate, they should be curtailed. The same should apply to Existing Use Permits. More detailed recommendations to promote EV use are provided in later sections.

IV. EV Chargers Installed in Public Lots

There are many benefits that come with installation of public EV charging stations. Owners of EVs will preferentially shop in locations with charging stations and the presence of these stations can attract outside customers into a new place in order to charge. It should be noted that no charger is instantaneous; drivers need something to do while charging.

Currently, the Township has two public Chargepoint CT4000 charging stations. The charging stations are located in the municipal library parking lot at 8900 Hawthorne Lane in Wyndmoor, Pennsylvania, in the vicinity of the township administrative building. Although there are only two active chargers available at the station, infrastructure was put in place to provide for easy expansion of the station at the municipal campus. From the time of installation through October 2020, the chargers have expended 1.108 MWh of energy in a total of 127 charging sessions, thus avoiding 465 kilograms of GHG emissions.



Funding for this program was provided through the Pennsylvania Department of Environmental Protection (PADEP) Level 2 EV Charging Rebate Program. Approximately \$7.7 million is being allocated over a 5-year period to fund this rebate program for the installation of Level 2 EV charging equipment. Rebates will be provided for Level 2 EV charging equipment for:

- Public use at government owned property
- Public use at non-government owned property

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- Non-public use at work places
- Non-public use at multi-unit dwellings

Pennsylvania organizations that submit complete and eligible applications will be provided rebate vouchers on a first come, first served basis. All projects for which a rebate is requested must be approved in advance by the PADEP. Complete applications that meet eligibility criteria will receive a voucher to hold their rebate funding for their project.

V. Getting to 100% Clean Transportation

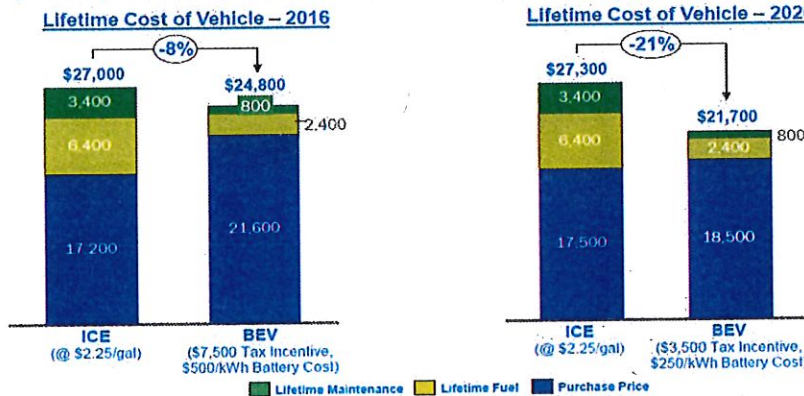
The most recent and comprehensive assessment of the energy usage and emissions of GHGs in southeastern Pennsylvania was conducted for the year 2015 by the Delaware Valley Regional Planning Commission (DVRPC). Based on the 2015 inventory, the emissions from energy use in buildings and in transportation together account for 95% of the GHG emissions in Springfield Township. Aside from reducing our generation of solid waste, virtually all of the GHG emissions in the township come from building energy usage and highway vehicles.

In order to eliminate mobile sources of GHGs, the miles driven per person must be reduced and both public and private vehicles must transition to non-fossil fuel sources. A variety of alternative fuel sources have emerged in recent years, including plant-based fuels, hydrogen fuel cells, and electric batteries. As outlined in the PADEP's [Drive Electric Roadmap](#), EVs are the alternative fuel vehicle of choice. After accounting for fuel cost savings, an electric vehicle over its lifetime is comparable to or costs less to own than a fossil fuel vehicle (see graphic below). In order to achieve the required degree of ownership of EVs in Springfield Township to reduce GHGs, infrastructure must be installed to facilitate charging.



EV Total Cost of Ownership (TCO)

Today EVs offer a lower TCO when compared to a traditional ICE vehicle, as battery prices fall EV purchase prices will be on par with ICE vehicles, making the value proposition even more attractive



Notes: BEVs include 24 kWh battery; IRS Code - ICT of \$2,500 for a vehicle battery with at least 5 kWh of capacity, plus \$417 for each kWh of battery capacity in excess of 5 kilowatt hours with a max of limited to \$7,500
Sources: US Internal Revenue Service, Gas Buddy, Accenture Analysis

Source: EP-ACT "TOSITO 2016" PECO 2016 Presentation "PECO Smart Driver Network Initiative"

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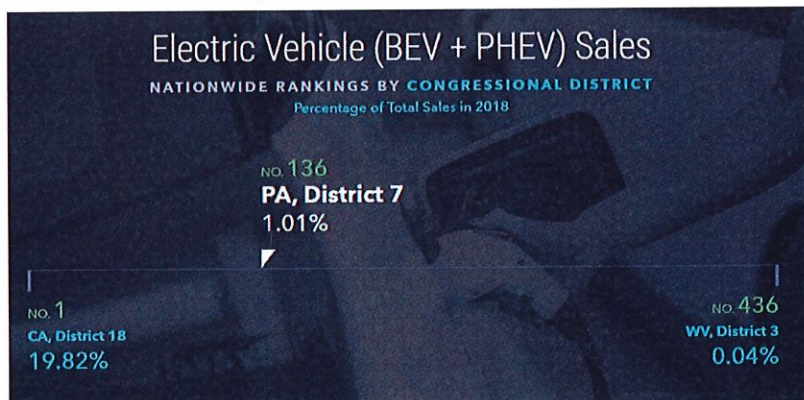
While vehicles comprise the majority of liquid fuel use in Springfield, other small combustion engines should be phased out across the township as well. These include lawn mowers, leaf blowers, and gasoline-powered portable outdoor lighting. These small engines contribute noise and disproportionately high levels of particularly dangerous air pollution ([EPA](#)) -- with estimates of an hour of lawn mowing releasing the same amount of pollution as 100 to 350 miles of car travel. Strategies to phase out these small combustion engines should also be developed.

In general, to eliminate GHG emissions from highway transportation, the following strategies must be applied within our township and beyond:

- Reduction in miles traveled through increased use of mass transit and non-vehicular travel (e.g., bicycles and walking), as well as compact land use patterns and tele-commuting.
- Increased fuel efficiency in ICEs.
- Replacement of ICE vehicles with EVs and hydrogen vehicles (primarily expected for use in heavy duty vehicles). An EV is 68% more efficient than an ICE vehicle because it converts the power to locomotion with considerably more efficiency than an ICE vehicle.
- Transition of the grid that supplies electricity to vehicles and equipment to 100% renewable, fossil fuel-free sources.

V.a. Strategies to Encourage Clean Transportation for Residents and Business Owners

While existing EV owners generally report high levels of satisfaction with their vehicles, the vast majority of consumers still have little understanding and many misconceptions about the capabilities and advantages of EVs, how they operate, and the nature of the charging/fueling experience. It's not surprising then that most consumers are not considering an EV for their next car purchase or lease. Pennsylvania has committed to transitioning vehicles to electric through the Drive Electric program. A number of resources are available discussing technology and strategies to build-out infrastructure and to encourage uptake of EVs ([PAEV Roadmap](#) and [DEP EV information](#)), and where a summary of current EV technology can be found. According to the Alliance of Automobile Manufacturers survey of EV sales in 2018, our Pennsylvania district (PA District 7) ranks 136 out of 436 in the country for EV sales (see [In Your State | Alliance of Automobile Manufacturers](#)).



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We recommend that Springfield Township take the following steps to encourage an increase of private EV purchase:

- *Create policies to enable community-wide adoption of electric vehicles:*
 - Review existing state and municipal ordinances related to EV readiness and EV charging.
 - Stay informed on grants available to fund EV transition (e.g., PADEP's Alternative Fuels Incentive Grant and the Volkswagen Zero Emission Vehicle Fund).
 - Adopt EV-ready building codes for new one- and two-family homes.
 - Adopt ordinances requiring a minimum percentage of EVSE-ready parking spaces in new or re-constructed residential and commercial parking structures.
 - Revise residential and commercial building codes to require supporting electric infrastructure for EVSE in new construction.
 - Mandate EV readiness for ≥50% of non-residential parking spaces. For example, this would include mandating that 20% of new parking spaces be charger ready (run conduit) and right-size electrical service for additional 40 Amp circuits for commercial and industrial accounts.
 - Identify ways to incentivize businesses and multi-unit dwellings to install chargers (e.g., tax abatement rules, zoning relief requests).
 - Review and update permits for EV chargers for residents.
 - Determine the current permitting process to install a charger and work to streamline this.
 - Put information on the process on the Township website/Facebook page and in a brochure.
 - Simplify EV charger permitting for commercial and industrial sectors.
 - Adopt no idling ordinances.
 - Coordinate EV charging infrastructure development with state plans, especially fast charge plans.
- *Support community-wide adoption of electric vehicles:*
 - Support the brand-neutral **Drive Change. Drive Electric.** campaign in partnership with automakers and work to bring in additional partners, highlight the expanding network of zero-emission vehicle charging and fueling infrastructure, and amplify the campaign messages and materials.
 - Make strategic investments in highly visible Level 1 and Level 2 chargers in state- and municipally-owned parking lots in central areas and at popular destinations (e.g. parks year-round).
 - Promote private investment in EVSE in central areas or parks with low-cost, long-term leases or no-cost use permits.
 - Deploy Level 1 and Level 2 charging stations in parking lots and garages located at high-traffic areas, and encourage private sector investments at these locations with long-term low-cost leases or no-cost use permits.
 - Deploy visible and easy to follow signage along access routes to charging stations and on site, respectively, to guide EV drivers to charging locations.

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- Expand EV charging at schools, municipal lots, workplaces, multi-unit housing (Level 1 and Level 2 and DC fast charge at transportation corridors).
- *Promote community outreach to support adoption of electric vehicles:*
 - Organize events in collaboration with EVSE providers, automakers, business-focused NGOs, business associations, and other partners to provide high-level recognition for business leaders that commit to investments in workplace charging and fleet electrification, such as the “Drive the Dream” events held in California and Vermont.
 - Offer workshops, webinars and educational materials for employers to support decision-making on workplace charging and promote efficient and high utilization of charging stations.
 - Convene stakeholders to encourage and expand EV charging at schools, municipal lots, houses of worship, workplaces, multi-unit housing (Level 1 and Level 2 and DC fast charge at transportation corridors).
 - Promote participation in [National Drive Electric Week](#) (NDEW) to learn from EV drivers and owners. Promote NDEW to town commercial, industrial sectors, schools and residents.
 - Implement an EV financial benefits awareness campaign (e.g., [Plug In America](#), NDEW, and Drive change Drive Electric campaigns).
 - Offer and promote existing state rebates, income tax credits, and sales and excise tax exemptions, and consider new programs and outreach efforts that could improve access to incentives among moderate and lower income consumers. For example: expanding eligibility to used vehicle purchases.

A key to reducing the need for vehicle use is to create **walkable and bikeable neighborhoods**. A walkable neighborhood is one in which residents are able to accomplish many of their daily activities without a car. Walking and biking are pleasurable, safe, and the transportation of choice. Such neighborhoods typically have a mixed use center with all of the following in a relatively small area:

- Shops and restaurants (including stores for everyday necessities like hardware and groceries)
- Localized community resources (neighborhood library, community center, public pool, school, parks and recreation)
- Offices/jobs
- A variety of housing types
- Good public transportation to connect neighborhoods and jobs

Transitioning Springfield Township to a more walkable series of neighborhoods centered around business districts in Erdenheim, Flourtown, Oreland and Wyndmoor would likely improve public health, reduce vehicle use, boost the local economy and real-estate value, while bringing neighbors out of their cars and face-to-face. *Section VI* below outlines other strategies and considerations for improved walkability and bikeability, including a list of existing and planned trails and trail circuits.

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V.b. Strategies to Transition to a Clean Municipal Fleet

Municipalities seeking to electrify their fleets must consider several factors, such as the best applications for EVs, ensuring adequate charging infrastructure, and training fleet drivers. This plan focuses on the adoption of light-duty zero emission vehicles; however, promising new developments in the application of zero emission technologies to medium- and heavy-duty transportation will complement the development of the light-duty (e.g., passenger vehicles) EV market by increasing demand for advanced batteries and chargers, and by raising public awareness. As with light-duty vehicles, continuing progress in reducing production costs (for both batteries and fuel cell systems) will continue to translate to real-world improvements in affordability and total operating cost. Additionally, savings related to EVs could provide a favorable acceptance of the technology by township residents, and *should be shared with the public*.

Springfield has limited resources; there are no surplus vehicles in the police fleet, therefore long charging times are a constraint. The recent commitment taken by the township to purchase hybrid police patrol cars is echoed by similar purchases in neighboring townships, and will create confidence in making future purchases of HEVs and AEVs based on comfort with the technology and savings in fuel usage, primarily during idling. The demands of police / highway vehicles and their cargo may limit the types of immediate improvements possible in the fleet.

In addition to purchasing more efficient or electric vehicles, there may be operational changes that can be made. It may be advisable for the township to investigate the number of hours each vehicle spends idling to see whether there are opportunities to reduce this. For most automobiles, idling for more than ten seconds wastes more fuel than simply turning off and restarting. Also, many studies have shown that frequent restarting has little effect on the mechanics of an automobile: excessive idling can result in incomplete combustion and can damage engine components, including cylinders, spark plugs, and exhaust systems.

Research on EVs, including available technology and financial assistance, during the municipal procurement process is central to the inclusion of hybrid and electric-only vehicles and equipment in the township fleet. There are sources of information on grants, EV technology, and municipal procurement that would help with the procurement process, such as the [Climate Mayors Electric Vehicle Purchasing Collaborative](#), the PADEP [Energy Programs Office](#), and the Pennsylvania Drive Electric program ([PAEV Roadmap](#)). A particularly useful resource is the [Electric Vehicle Resource Kit for Municipalities](#) prepared by the DVRPC: this toolkit is targeted to municipal managers and addresses concerns related to the purchase and incorporation of EVs into municipal fleets in Pennsylvania and New Jersey. Other recommendations for transition to a clean municipal fleet are listed below.

- Conduct Fleet Vehicle Audits to assess the availability, costs, benefits, and barriers to utilizing alternative fuels and vehicles, and help to identify other fuel-saving opportunities.
- Retain an EV integration company to accelerate town fleet economic, energy and emissions savings through planned fleet decarbonization.

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- Conduct and keep an updated, detailed GHG inventory of transit-related emissions in the township.
- Advance electrification of the fleet by setting municipal near- and long-term electrification goals:
 - Develop a 10 year fleet decarbonization plan and process
 - Raise awareness and acceptance of EVs by offering information, maintenance training and opportunities for test drives to fleet administrators
 - Conduct fleet-wide inventories of vehicles that could be replaced with EVs; quantifying potential fuel and maintenance cost savings and encouraging vehicle selection based on total cost of ownership; and, assessing opportunities to secure the benefit of the federal electric vehicle tax credit through leasing or other means
 - Lead by example by installing workplace chargers for full-time employees
 - Explore innovative ways to reduce the total capital and operational costs of in-fleet charging stations, for example, through EVSE sharing agreements with other public institutions and private businesses
- Consider cooperation with other municipalities, such as collaboration on EV-related events and in development of an EV and EV charger adoption roadmap to save \$100 towns in TCO compared to driving as usual.
- Establish Municipal Procurement policies to purchase low GHG-emitting vehicles to replace existing or “retiring” conventional, fossil-fuel vehicles.
- Set energy efficiency and fuel economy goals for the municipal fleet (e.g., 50 mpg by 2025, 100 mpg by 2028).
- Advance electrification of the municipal fleet through employee education.
 - Municipal fleet representative participates in National Drive Electric events
 - Promote driver behavior and/or incorporate technologies to reduce vehicle engine idling and other best practices
 - Reward workplace charging (Workplace chargers significantly increase EV miles traveled (eVMT); employees are 20 times more likely to drive EV if they can charge EVs at work.)

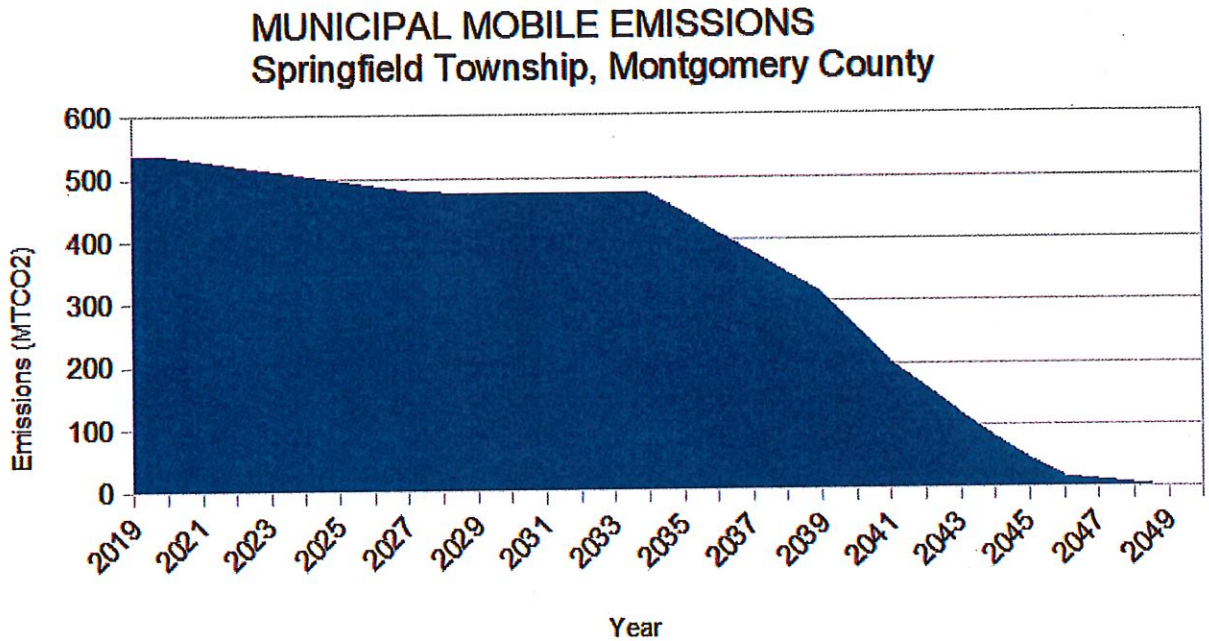
The following steps provide a **path toward ZERO municipal mobile emissions** in Springfield Township. As noted previously, emergency vehicles are not included in the emission reduction strategy as the township does not have ownership of these vehicles; in addition, vehicles and equipment owned by the School District of Springfield Township are also not addressed here.

1. Police patrol vehicles will be replaced by hybrid vehicles (HEVs), two vehicles per year, starting in 2021. Administrative vehicles will be replaced with retired police vehicles.
2. Police patrol vehicles will be replaced by electric-only vehicles (BEVs), two vehicles per year, starting in 2035 (or earlier). The year 2035 was chosen conservatively, as this year is the township deadline for transitioning municipal sources of electricity to 100% renewable. Administrative vehicles will be replaced with retired police vehicles.
3. Gas-fired Public Works vehicles and equipment (i.e., pick-up style trucks, dump trucks, loaders and tractors) will be replaced by electric-only vehicles and/or equipment, at the equivalent of one vehicle per year, starting in 2035 (or earlier).

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4. Diesel-fired Public Works vehicles and equipment, with the exception of refuse vehicles, will be replaced by electric-only vehicles and/or equipment, at the equivalent of two vehicles per year, starting in 2035 (or earlier).
5. Refuse vehicles will be replaced by electric-only vehicles, one vehicle per year, starting in 2040 (or earlier).

This conservative, staggered approach allows for a trickle-down of available, affordable technology to a smaller municipality such as Springfield Township. These strategies are non-binding and representative, but serve as a guide for achieving a 100% clean fleet by 2050. *The most important aspect of this approach is to include research and review of alternative fuel vehicles and equipment in the township's procurement process.* The chart *Municipal Mobile Emissions* provided below is a representation of the emissions reductions achieved by this strategy.



Small equipment, which is gasoline- or diesel-powered, generates enormous amounts of toxic air pollution. Steps should be taken to transition this equipment to electric and to reduce the need for it, including:

- Reduce mowing: Areas of parks which are not used for recreation and which could be converted to meadow should be actively identified and steps taken to make the transition.
- Replace lawn care equipment with electric models as available; otherwise, purchase the lowest emissions models.

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In order to achieve ZERO carbon emissions, full electrification of the municipal fleet is a necessity **in combination with** achievement of 100% renewable energy. Emissions of EVs are only as good as the source of its electricity. The U.S. Department of Energy Alternative Fuels Data Center has a useful tool to determine how the source of your electricity has an effect on the carbon footprint of your electric vehicle (see https://afdc.energy.gov/vehicles/electric_emissions.html).

VI. Moving Forward

Based on experience in other neighboring communities, reducing the necessity of private car use in Springfield will improve property values, grow the local economy, reduce air pollution, promote health through physical activity, strengthen a sense of community through increased human interactions, and create a more equitable society. The basic structure of neighborhoods organized around mixed use and commercial districts, and connected by public transit, exists in our township. The Township should take clear steps to develop these neighborhood commercial and mixed-use districts and to enhance non-car modes of transportation within and between these neighborhoods.

VI.a. Existing and Planned Trail Systems

Although Springfield Township is a small municipality compared to surrounding townships, it offers access to many different area trails and trail systems:

Piszek Preserve, Wissahickon Trails (formerly Wissahickon Valley Watershed Association). This is a relatively short paved trail that is located within the township on the western side of Pennsylvania Avenue in the Oreland section of the township. The paved trail extends from the newer housing developments of Springfield Manor to Penn Manor. This area is frequently flooded during intensive storm events as Sandy Run Creek overruns its banks; Sandy Run Creek empties into Wissahickon Creek. This trail has limited access by foot or bike. There is a visitors parking lot located at Springfield Manor; however, even local residents along Pennsylvania Avenue cannot safely walk to this trail due to lack of sidewalks along Pennsylvania Avenue and the high traffic risk from crossing Pennsylvania Avenue to get from the eastern side of the Preserve (largely located within Upper Dublin Township) to the western side.

<https://wissahickontrails.org/explore-the-watershed/find-your-trail/piszek-preserve>

Sandy Run Park, Springfield Township. Separated from Piszek Preserve by the train tracks serving the SEPTA Lansdale-Doylestown Regional Rail Line, this 14-acre park can be found on Walnut Avenue in the Oreland section of the township. It contains an abandoned quarry that was filled with water many years ago. The township is planning to place a walking trail by Sandy Run Creek and Sandy Run Country Club to better connect walkers from Oreland to the Sandy Run Park. Other notable municipal parks with existing or future walking trails located within Springfield Township include **James A. Cisco Park** on Montgomery Avenue and **Mermaid Park** on Mermaid Lane.

<https://www.springfieldmontco.org/departments/parks-recreation/parks-facilities/>

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In addition, a planned use of the site of the former Enfield Elementary School located at Church and Paper Mill Roads in Oreland may include walking trails along the perimeter of the property. This property is owned by the School District of Springfield Township.

Green Ribbon Trail, Wissahickon Trails. As noted on the trail's website, this trail begins at Parkside Place in Upper Gwynedd Township and ends at Stenton Avenue in Whitemark Township. It is over 12 miles in lengths and shadows the Wissahickon Creek for this entire stretch. Although this trail does not fall within the township borders, there are several local access points, including along West Valley Green Road and at the **Fort Washington Park Flourtown Day Use Area** on West Mill Road. This trail system also provides access to and/or through several of the preserves owned and managed by Wissahickon Trails. Pizek Preserve, described above, is also owned by Wissahickon Trails, but is not currently connected to the Green Ribbon Trail. Wissahickon Trails is a non-profit organization that preserves land for public access; there is a long-term commitment to connecting all of its trails and preserves, as well as connecting to other area trail systems (such as that in Wissahickon Valley Park in Philadelphia).

<https://wissahickontrails.org/explore-the-watershed/find-your-trail/green-ribbon-trail-2>

There is a planned **Wissahickon Trail Connector**, which will provide a missing segment to connect the trail located at Forbidden Drive in the Wissahickon Valley Park (see below) to the Green Ribbon Trail end segment found at Stenton Avenue (in Whitemarsh Township). Plans to connect and expand existing trails with Montgomery County can be found in the [*MontCo 2040: A Shared Vision*](#) plan.

Wissahickon Valley Park, Friends of Wissahickon and Philadelphia Parks & Recreation. This park, located within the City of Philadelphia, comprises 1,800 acres of land that follows Wissahickon Creek from the intersection of Germantown and Northwestern Avenues down to Lincoln Drive and the outlet of the creek into the Schuylkill River. There is walkable/bikeable access for township residents that live along the Lafayette Hill panhandle of the municipality, while other residents may drive and park at access points on Northwestern Avenue and at Bells Mill Road.

<https://fow.org/visit-the-park/>

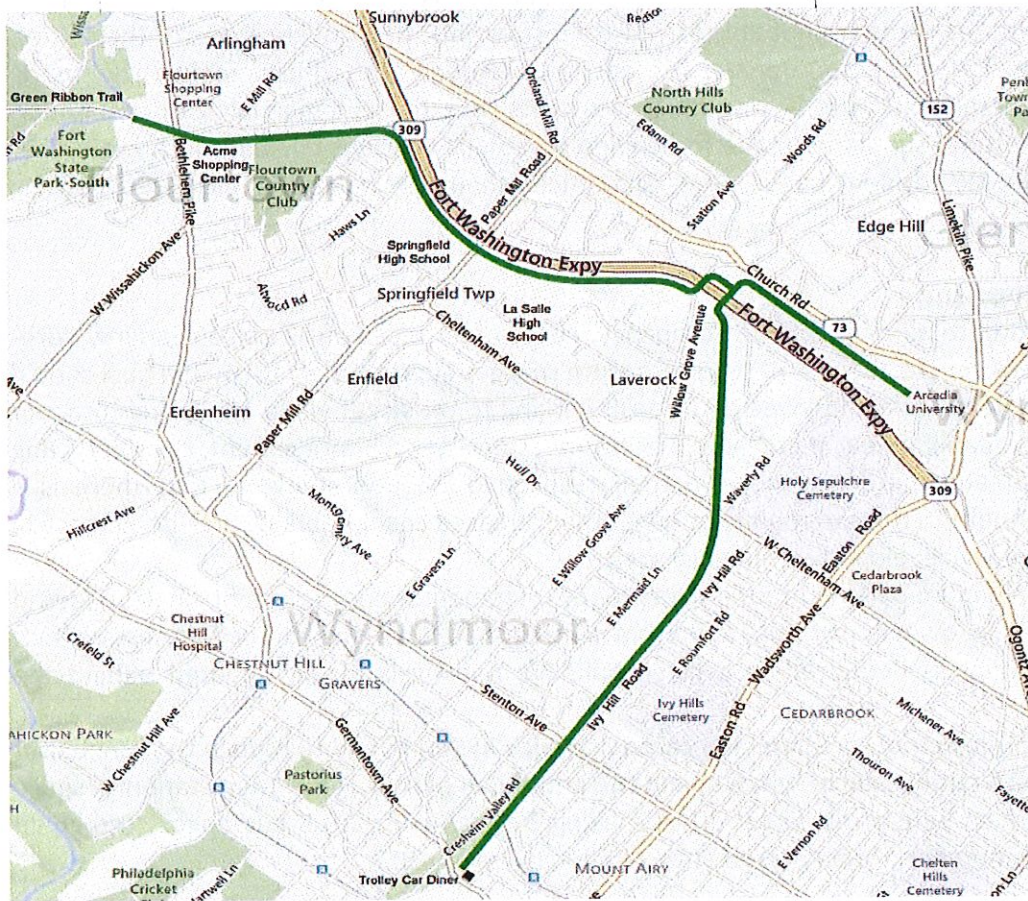
Fort Washington State Park, Pennsylvania Department of Conservation & Natural Resources. Although most of this Pennsylvania state park is located outside of township borders, there are several local access points to this park (and a portion of the Green Ribbon Trail within the park). Walkable/bikeable access to this park and all of its resources, camping sites, and trails can primarily be found to the southernmost portion via the Flourtown Day Use Area off West Mill Road. As noted above, the Green Ribbon Trail extends far to the north of the township.

<https://www.dcnr.pa.gov/StateParks/FindAPark/FortWashingtonStatePark/Pages/default.aspx>

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Cresheim Trail, The Cresheim Trail is part of a 750-mile multi-use trail circuit within the Greater Philadelphia region, which includes Montgomery County. There is planned expansion (see map below) of this trail system that would potentially connect the Green Ribbon Trail at the southern portion of Fort Washington Park, along Route 309 within the township, and then through the corridor between Mermaid Lane and Ivy Hill Road in the Wyndmoor section of Springfield Township. This stretch in Wyndmoor would potentially be linked to Mermaid Park, a municipal park that will be upgraded with its own small trail circuit around an existing pond. This proposed expansion of the Cresheim Trail section would connect to the existing trail along Cresheim Valley Road in Philadelphia (and the Fairmount Park system located within the city). A figure showing the proposed expansion is provided below.

<https://cresheimtrail.org/>



Morris Arboretum, University of Pennsylvania. This 92-acre garden is partially located within the western “panhandle” section of the township along Northwestern Avenue. Although the Arboretum may be within walking/biking distance for some residents, it does require a fee to gain access.

<https://www.morrisarboretum.org/>

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Many of the trails described above are part of the **Circuit**, which is a 750-mile network of multi-use trails planned for the Greater Philadelphia region.

VI.b. Recommendations for Township Planning

The following recommendations address urban planning and walkability.

- **Put cars in their place.** Adopt a resolution supporting non-car-based transportation and recognizing that building bigger streets does not improve congestion but increases traffic. This resolution can guide new road and development projects.
- **Increase mixed use districts.** In a primarily residential community like Springfield, it is almost mandatory to own a private vehicle to complete the ordinary tasks of daily living. With only a few exceptions, residents are not able to walk to restaurants, the grocery store, or the doctor. Areas of the Township appropriate for adding additional mixed-use zoning should be identified. Where mixed use zoning currently exists, but the commercial uses are under-represented, efforts should be made to develop these often vacant buildings into businesses serving day-to-day resident needs. Also, allowances should be made for increased housing density.
- **Get the parking right:**
 - Price parking to allow for ~85% occupancy, coordinate prices of off-street parking, curbside parking, and residential permits to ensure space is appropriately used. The goal should be to limit long-term employee and transit parking to off-street while increasing turnover of curbside parking near merchants. For future street-scape improvement, consider using parking lot and meter revenues in their respective neighborhoods so that there is a positive benefit to the community to offset the increased cost of parking and/or increase in traffic in newly developed commercial areas.
 - Remove minimum parking requirements for development. Consider use of “in lieu” fees in which building owners pay a fee towards a central, shared parking lot in a business district. Where parking is dedicated to a given building, parking is ideally situated behind the building.
 - Consider an (electric) car share and carpool service in Springfield to reduce the number of cars that are rarely used or which spend the entire day parked at the train station. A single carpool driven by a professional driver making 3 trips/hour with 3 passengers/trip could reduce the number of cars parked at the station by nearly 30.
- **Let transit work:** Springfield Township, in collaboration with other Montgomery County communities, must work with SEPTA to improve public transit services.
 - More **frequent rail service** should be restored to Oreland, North Hills, Wyndmoor and Chestnut Hill train stations. Commuters who may have gotten on at these small neighborhood stations are now forced to drive longer distances and park at other stations.

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- A limited number of bus routes currently run approximately 1/hour. This effectively limits ridership to those who cannot afford cars, forcing economically disadvantaged citizens to have hours-long commutes to travel short distances. More and better connected routes with access to places that residents want to go should be added. Service should run no less frequently than every 10 minutes. Transit pass pricing should be more equitable, with discounts for children and/or families and the economically disadvantaged. Use small (electric) buses which can be more easily filled with people and fit better on our narrow streets. If SEPTA refuses to consider these changes, find a private vendor to pilot service in Springfield either alone or with neighboring communities.
- Create **transit-oriented development** overlay districts surrounding the Oreland regional rail train station to encourage people to “live, work, and play” near transit, further reducing the need for private cars.
- **Protect Pedestrians:**
 - Work with PennDOT and other **road planners** to ensure that intersections and street widths are appropriate for the surrounding neighborhoods. Excessively wide, straight streets have been shown to increase speeds and create a sense of being unsafe for pedestrians and cyclists. Converting the curb lane to parking, tree plantings or a bicycle lane where appropriate will protect pedestrians, reduce car speeds, and promote cycling. Narrowing lanes in residential neighborhoods could allow for addition of bicycle lanes and will slow cars.
 - The most walkable cities have small blocks or large blocks with many **pedestrian cut-throughs**. While Springfield’s blocks are mostly established, this should be considered in any new developments. For existing long blocks, work with neighbors to identify key places where a bridge or a path could greatly shorten a long walk-around and allow pedestrians to walk on a quieter path. *It should be noted that the Springfield residents that live along the Pennsylvania Avenue corridor are disconnected from the township at-large by the train tracks that serve the Lansdale-Doylestown SEPTA line.*
 - Ensure pedestrian crossings are safe. At busy intersections, reduce right on red (especially continuous right turns with yield sign), and implement Leading Pedestrian Interval (LPI) which gives pedestrians approximately 3 seconds to start walking before cars get the green light to turn. This improves visibility of the pedestrian to the turning motorist. Add crossings where they currently do not exist.
 - Develop a **public information campaign** coupled with signage and ticketing to modify motorist behavior regarding crosswalks.
 - Develop a plan to provide **sidewalks** and safe routes for pedestrians where needed in the township. An excellent resource for analyzing the availability of pedestrian-safe zones is the DVRPC [Pedestrian Portal](#). Also, the PennDOT page contains information related to training and funding to create [Safe Routes to School](#).
- **Welcome Bikes:** Adding cycling infrastructure to increase cycling activity has been shown to benefit the economy, attract new residents, increase housing prices, and improve health. In

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addition, as the number of cyclists in a community increases, motorists become more cautious, speeds are reduced and pedestrian and cyclist safety improves.

- Communicate all road improvement plans to Montgomery County regional planning prior to finalization; their bike infrastructure specialist can provide input regarding bike facilities.
 - Through community meetings in each Ward, identify areas for improved bike (and pedestrian) infrastructure.
 - Invest in a professional planner to develop an integrated biking plan (both on road and multi-use trail) for the Township. Ensure connectivity with surrounding communities, the county, and the local and adjacent trails.
 - Ensure that new developments incorporate appropriate biking infrastructure.
- **Plant Trees.** In Philadelphia, there is a 20-degree difference in temperature between affluent and poor neighborhoods in summer. This temperature difference is linked to the presence of more trees in affluent neighborhoods. In addition to shade, trees absorb substantial storm run-off, clean the air, absorb GHG emissions, and provide Springfield with one of its unique characteristics.
 - With the help of the Shade Tree Commission and community groups, identify streets and parks that need to have trees added or replaced.
 - Create a team consisting of neighbors and organizations focused on tree planting (e.g., Audubon, Tree Tenders). Develop a plan and identify funding to purchase, plant, water, and tend these trees.
 - Native trees should be used, with species selection favoring those more resilient.
- **Make Friendly and Unique Faces:** Commercial development should be done to support street life and make people want to get out of their cars. Interesting and inviting design can make a long walk feel shorter. In addition, creating a sense of an enclosed space rather than a wide open one is psychologically more attractive to humans.
 - Parking should be as invisible as possible. Walking past a large strip of asphalt is both boring and, in summer, scorching. Surface parking should be behind buildings, greenery or other structural façade. Parking structures should be masked so that they resemble occupied buildings.
 - Shop fronts should be close to the street. Large canopies or trees should be used to provide shade. Seating areas and benches should be used throughout.
 - An example code from Melbourne, Australia, states that 60% of street facades should be open and inviting (e.g., windows instead of blank walls).
 - Consider transitioning to a form-based code rather than traditional zoning. (See www.transect.org/codes.html.)
 - Keep green in its place. Springfield's preponderance of trees and pocket parks are one of its best features. However, to encourage walking as a form of transportation, it is important to create lively business districts that may be lacking in extensive green spaces. A building set close to the street without a front yard is more appropriate for a lively commercial area.

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- **Prioritize and plan.** Not every neighborhood can be adapted for walkability in its current form. Instead, Springfield should focus on redeveloping and improving the existing commercial districts surrounded by relatively dense housing, particularly those located near transit. Within these districts, anchor stores and structures should be identified, with creation of walkable paths connecting these being the first priority. As new development comes to Springfield in the future, principles of walkability outlined previously can be applied.

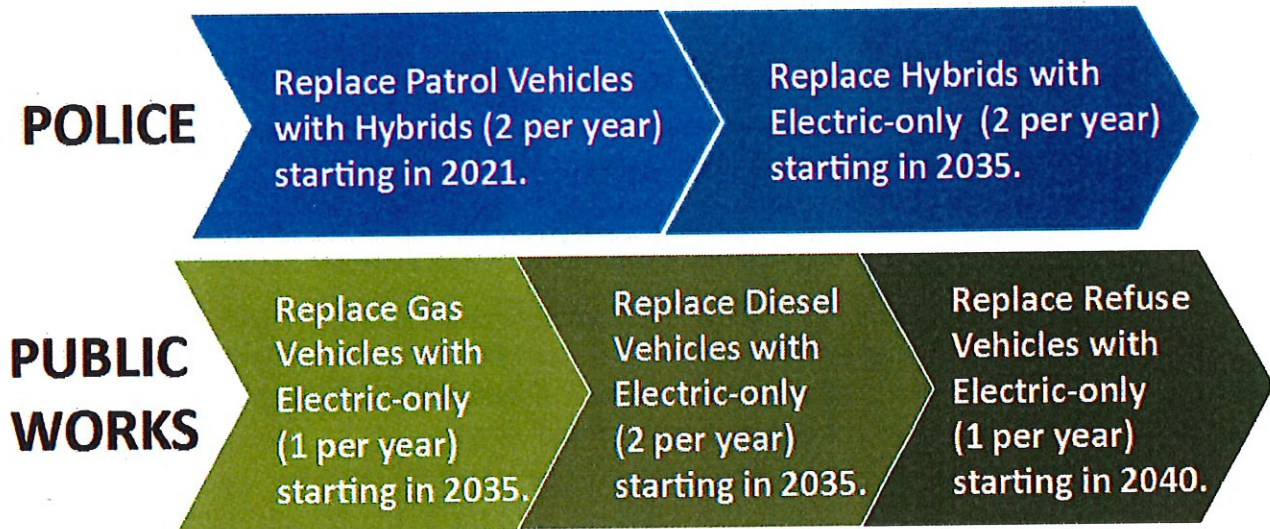
- **Encourage Up-take of Private EV Ownership.** The Township should take the following steps to encourage up-take of private EV purchase.
 - Stay informed on grants available to fund EV transition, such as the PADEP [Alternative Fuels Incentive Grant](#) and the [Volkswagen Zero Emission Vehicle Fund](#).
 - Remove Permitting/Zoning Barriers for publicly available charger installation (i.e., pay-to-charge):
 - Identify where in the Township such facilities would be permitted under current zoning.
 - Determine and streamline the current permitting process to install a charger.
 - Put information on the process on the Township website and in a brochure.
 - Identify ways to incentivize businesses and multi-unit dwellings to install chargers (eg, tax abatement rules, zoning relief requests).
 - Remove Permitting/Zoning Barriers for private charger installation:
 - Determine the current permitting process to install a charger and work to streamline this.
 - Put information on the process on the township website and in a brochure.
 - Lead by example: As described in prior sections, purchase EVs for municipal vehicle replacements wherever possible. Install additional Level 2 chargers at visible locations in the township funded in part by the PADEP rebate.
 - Encourage installation of public chargers in privately owned lots:
 - Identify key parking lots to target (ensure currently zoned commercial)
 - Send invitation letter and brochure on the PADEP rebate process from the Township
 - Consider offering tax incentives
 - Consider offering township help with rebate applications
 - Look into permitting process for electrical work
 - Develop a plan for homeowners with no off-street parking. Examples of approaches include:
 - Allowing private chargers in the right-of-way, similar to requesting a handicap spot.
 - Modify ordinances and/or work with the Zoning Committee to allow installation of chargers in under-used neighborhood parking lots (e.g., church parking lots) which could provide ready access to charging for those without off-street parking.
 - Consider allowing public networked chargers on residential streets.
 - Adopt pro-EV Building Codes:

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- Adopt codes that require new residential and commercial construction to be pre-wired for chargers (this is less expensive than retrofits later).
- Increase the proportion of chargers required per number of spaces.

A staggered, conservative approach for transitioning the municipal fleet to 100% clean energy is discussed in Section V.b and depicted in the graphic below. There is no commitment to the approach laid out in this Energy Transition Plan: the overarching goal is to reach a 100% clean fleet by 2050. The difficulty in planning the details of the fleet transition at this time is the knowledge that the technology and availability of electrified vehicles and equipment will likely change quickly over the next few decades. Regardless, the township procurement process should include research into EVs and electric equipment as the opportunity arises.

Electrification of the Springfield Township Municipal Fleet



Electrified transportation, including electric vehicles, is expected to play an increasingly important part in regional transportation planning, as well as abatement of the effects of climate change. Electrified transportation and equipment offer the following benefits:


- Greenhouse gas reduction, which is furthered as the energy grid transitions away from fossil fuels
- Improved air quality and, thus, improve public health and lower health-related costs
- Cost savings over liquid fuel, making money available which may be invested back into the community
- Expected to help stabilize the renewable-supplied electrical grid
- Incorporate measures in plans of economic and community development to attract EV drivers

The path forward to a clean, electric-only municipal fleet and walkable, bikeable communities starts with dedication among the leaders of our township and community.

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Glossary of Terms

AEV	All-Electric Vehicle
BEV	Battery Electric Vehicle
CO ₂	Carbon Dioxide
DC	Direct Current
DOT	Pennsylvania Department of Transportation
DVRPC	Delaware Valley Regional Planning Commission
EPA	United States Environmental Protection Agency
EREV	Extended Range Electric Vehicle
ETP	Energy Transition Plan
EV	Electric Vehicle
EVSE	Electric Vehicle Supply Equipment
FCEV	Fuel Cell Electric Vehicle
GHG	Greenhouse Gas (e.g., CO ₂ and methane)
HEV	Hybrid Electric Vehicle
ICE	Internal Combustion Engine (e.g., gas-fired)
LPI	Leading Pedestrian Interval
LPPU	Local Public Procurement Unit
mpg	Miles per gallon
MTeCO ₂	Metric Tons (of GHG emissions) equivalent to CO ₂
MWh	Megawatt hours
NDEW	National Drive Electric Week
NGO	Non-governmental organization
PADEP	Pennsylvania Department of Environmental Protection
PGS	Pennsylvania Department of General Services
PHEV	Plug-in Hybrid Electric Vehicle
SAE	Society of Automotive Engineers
TCO	Total Cost of Ownership
VMT	Vehicle Miles Traveled





SPRINGFIELD TOWNSHIP
ENVIRONMENTAL ADVISORY
COMMISSION

RENEWABLE SPRINGFIELD

Springfield Township's Transition to 100% Clean Energy

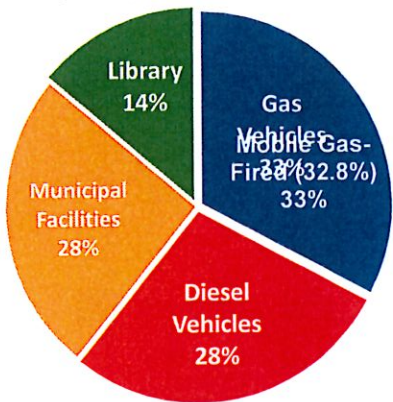
JULY 12, 2021



Township Transportation Costs

MUNICIPAL CARBON EMISSIONS BY SOURCE Springfield Township, Montgomery County

- 39,652 gallons of gasoline
- 26,788 gallons of diesel fuel
- Approximately \$125,000/annually
- Nearly 600 metric tons CO2 equivalent (MTeCO2)
- Mobile sources comprised the largest combined municipal source, **approximating over 60% of total emissions.**



Source	Percentage
Gas (Mobile Gas-Fired Vehicles)	32.8%
Diesel Vehicles	28%
Municipal Facilities	28%
Library	14%

4



A Greener Way to Travel

TRANSPORTATION



Clean transportation by 2050 involves a continuing conscious emphasis on energy usage and efficiency, not only with municipal sources of carbon emissions, but also among the larger community.

The EAC Clean Transportation Group focuses on the transition of the municipal fleet, currently energized by fossil fuels, to vehicles and equipment that obtains renewable energy from the electric grid.

The greatest effect in reducing carbon emissions from municipal sources lies in **electrifying all currently fossil fuel vehicles and equipment owned and operated by the township.**



Transitioning the Twp. Fleet

Non-binding, staggered approach that can be altered as needed

** does not include fire and ambulance vehicles*

POLICE

Replace Patrol Vehicles with Hybrids (2 per year) starting in 2021.

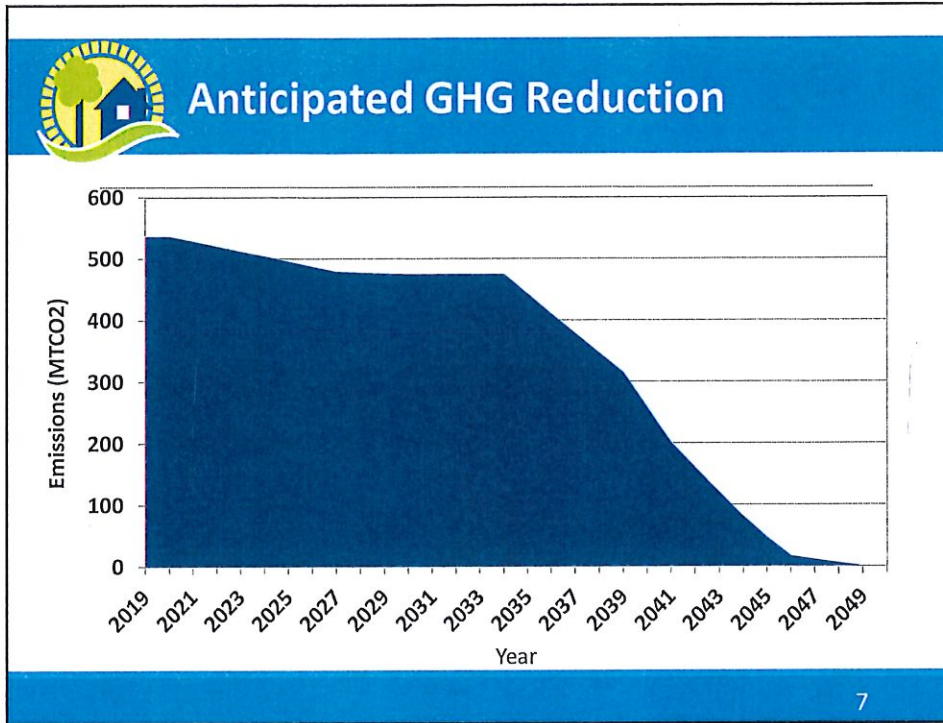
Replace Hybrids with Electric-only (2 per year) starting in 2035.

PUBLIC WORKS

Replace Gas Vehicles with Electric-only (1 per year) starting in 2035.

Replace Diesel Vehicles with Electric-only (2 per year) starting in 2035.

Replace Refuse Vehicles with Electric-only (1 per year) starting in 2040.



Other Recommendations

Municipal procurement process is central to the inclusion of hybrid and electric-only vehicles and equipment in the township fleet.

Encourage private EV purchase, such as promoting outreach and creating policies, to enable **community-wide adoption of EVs**.

Strive to make Springfield Twp. **more walkable/bikeable**, therefore, more sustainable and healthy. A walkable neighborhood is one in which residents are able to accomplish many of their daily activities without a car.

Per Jeff Speck, *Walkable City*: **How do you get people to walk?** Give them a reason, make it comfortable, make it interesting, most of all **make it safe**.



Increasing Ease of Going Green

The future of electrified transportation is a changing landscape.

A domino effect of manufacturers are signing on to varied pledges of electrification, including Volvo, Volkswagen, Ford, GM, Honda...

Federal commitments to building a charging infrastructure with 500,000 stations, investment of \$174 billion in transportation electrification.



FORD F150 LIGHTNING



Mack® LR electric REFUSE vehicle

**BID OPENING (via PennBid)
FEBRUARY 3, 2022**

**SCHOOL ZONE FLASHER INSTALLATION
PAPER MILL ROAD AND MONTGOMERY AVENUE
AT ENFIELD ELEMENTARY SCHOOL**

<u>BIDDER</u>	<u>BID AMOUNT</u>
Carr & Duff Inc. Huntingdon Valley, PA	\$124,959.00
Armour & Sons Electric Langhorne, PA	\$125,382.00
Miller Brothers Conshohocken, PA	\$154,181.49
Wyoming Electric and Signal Wyoming, PA	\$140,084.00

BID NOTICE

Notice is hereby given that the Board of Commissioners of Springfield Township, Montgomery County, PA, will accept sealed proposals for the following:

**SCHOOL ZONE FLASHER INSTALLATION
Paper Mill Road (SR 2032) and Montgomery Avenue,
Springfield Township, Montgomery County, PA**

Bids will be accepted until 12:00 PM, February 3, 2022 via PennBid (www.pennbid.net) only. Questions must be submitted via PennBid by January 28, 2021 at 4:00 PM.

Plans and specifications are available online at no charge via PennBid.

All proposals (bids) are to be accompanied by a bid bond or certified check in the amount of 10% of the total bid. The Township reserves the right to waive any minor informalities, irregularities or defects in the bid; or to reject any or all of the bids. No bid may be withdrawn within sixty (60) days after the opening thereof, during which time the Township may review the bids and investigate bidder qualifications. The successful Bidder must comply with Pennsylvania Prevailing Wage Act.

All Bidders are required to establish proof of competency and responsibility, in accordance with 67 PA Code Chapter 457, Regulations Governing Prequalification of Prospective Bidders and PennDOT Form 408/2020, as supplemented and amended, Section 102. Subcontractors must prequalify in the same manner, if listed in the proposal or if they subsequently undertake the partial or total construction of one or more items of work. All Bidders and Subcontractors must be PennDOT prequalified. The Contractor must ensure that employees and applicants for employment are not discriminated against because of their race, color, religion, sex, national origin or handicap.

Prevailing Wage determinations are in effect for this project as set forth in the Bid Documents.

Springfield Township does not discriminate on the basis of race, color, religion, national origin, age, handicap or sex.

A. Michael Taylor,
Township Manager

TRAFFIC SIGNAL NOTES

DO NOT MODIFY INSTALLATION WITHOUT PRIOR WRITTEN APPROVAL. ALL SIGNS AND PAVERS MARKINGS INDICATED ARE PART OF THE PERMIT. INSTALL AND MAINTAIN IN ACCORDANCE WITH SUBCHAPTER 22.02 PERMIT. MAINTAIN 8 FEET CLEARANCE OF CURB OR EDGE OF SHOULDER; AND 8 FEET ABOVE SIDEWALK OR PAVEMENT GRADE.

OVERHEAD SIGNALS: FIELD CLEARANCE OF 18 FEET MINIMUM AND 19 FEET MAXIMUM ABOVE ROADWAY; RIGIDLY MOUNT, TOP AND BOTTOM, AND EQUIP WITH ENCLOSURES; PROVIDE A MINIMUM 10-MINUTE HEADWAY BETWEEN SIGNALS AS MEASURED AT RIGHT ANGLES TO THE APPROACH.

DETERMINE WITH A PENNDOT REPRESENTATIVE, THE EXACT LOCATION OF DEEDERS PRIOR TO INSTALLATION.

ALL WORK SHALL BE IN ACCORDANCE WITH PENNDOT SPECIFICATIONS TO UNDERGROUND UTILITIES, DATED 1974.

CONVEY WITH PROVISIONS OF ACT 287 FOR PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED 1974.

ALL WORK SHALL BE IN ACCORDANCE WITH PENNDOT SPECIFICATIONS TO UNDERGROUND UTILITIES, DATED 1974.

THIS CONSTRUCTION PLAN WAS PREPARED FROM THE OFFICIAL PENNDOT PERMIT PLAN. COPIES OF THE SIGNED PERMIT SHALL BE FORWARDED TO THE PENNDOT OFFICE OF TRANSPORTATION. RESPONSE PERMIT NUMBER: 3-5447-02.

TRAFFIC SIGNAL CONSTRUCTION PLAN

COUNTY: MONTGOMERY.

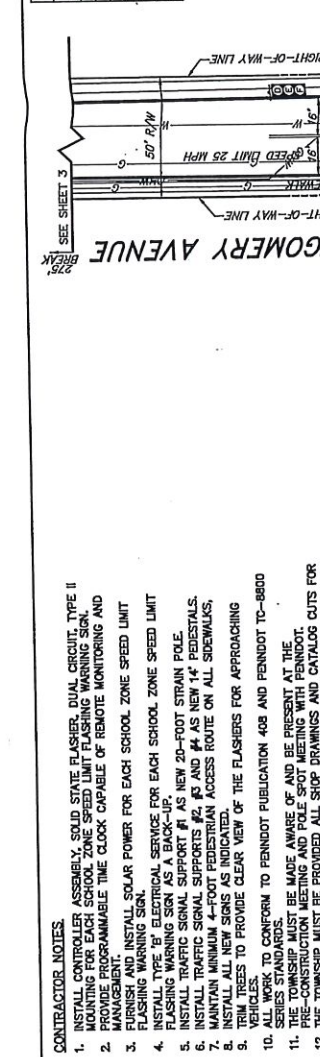
MUNICIPALITY: SPRINGFIELD TOWNSHIP.

INTERSECTION: PAPER MILL ROAD (SR 2032), MONTGOMERY AVENUE AND ENFIELD ELEMENTARY SCHOOL.

DATE OF ISSUE: 01/16/2022

NO.	REVISION	DATE
1		
2		
3		
4		
5		

1 OF 2 MCM PROJECT # - 821935.3A

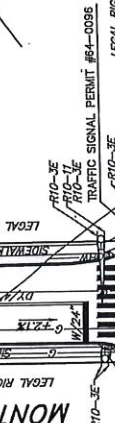


CONTRACTOR NOTES

- INSTALL OPERATIONAL ASSEMBLY, SOLID STATE, DUAL CIRCUIT, TYPE II FLASHING WARNING SIGN, AS SHOWN ON EACH SCHOOL ZONE SPEED LIMIT FLASHING WARNING SIGN.
- PROVIDE PROGRAMMABLE TIME CLOCK CAPABLE OF REMOTE MONITORING AND MANAGEMENT.
- FURNISH AND INSTALL SOLAR POWER FOR EACH SCHOOL ZONE SPEED LIMIT FLASHING WARNING SIGN.
- INSTALL SOLAR SERVICE FOR EACH SCHOOL ZONE SPEED LIMIT FLASHING WARNING SIGN AS A BACK-UP.
- INSTALL TRAFFIC SIGNAL SUPPORT #1 AS NEW 20-FOOT STRAIN POLE.
- MAINTAIN TRAFFIC SIGNAL SUPPORTS #2, #3 AND #4 AS NEW 14' FEDESTALS.
- MAINTAIN MINIMUM 4-FOOT FEDESTRIAN ACCESS ROUTE ON ALL SIDEWALKS.
- INSTALL ALL NEW SIGNS AS INDICATED.
- TRIM TREES TO PROVIDE CLEAR VIEW OF THE FLASHERS FOR APPROACHING VEHICLES.
- CONFORM TO PENNDOT PUBLICATION 408 AND PENNDOT TC-8800 SERIES STANDARDS.
- THE TOWNSHIP MUST BE MADE AWARE OF AND BE PRESENT AT THE PRE-CONSTRUCTION MEETING AND POLE SPOT MEETINGS WITH PENNDOT.
- THE TOWNSHIP MUST BE PROVIDED ALL SHOP DRAWINGS AND CATALOG CUTS FOR THE TOWNSHIP PERMIT.
- THE TOWNSHIP MUST BE GIVEN THE CONSTRUCTION SCHEDULE AND BE NOTIFIED 24 HOURS PRIOR TO WORK COMMENCING SO THEY MAY BE PRESENT FOR ANY SIGNIFICANT CONSTRUCTION (FOUNDATION POURING, CONDUIT TRENCHING, ETC.) AT THE INTERSECTION.

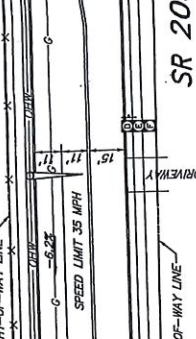
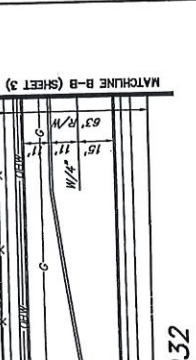
SIGN TABULATION

BY AN SYMBOL	SIZE	REMARKS
(A)	24"x30"	SCHOOL SPEED LIMIT FLASHER (SEE DETAIL)
(B)	36"x36"	END SCHOOL ZONE
(C)	36"x36"	SCHOOL
(D)	24"x30"	SCHOOL PANEL
(E)	24"x30"	SPEED LIMIT (15)
(F)	24"x30"	RESTRICTED HOURS BLADE (SEE SHEET 1)
(G)	24"x30"	SPEED FEEDBACK DISPLAY



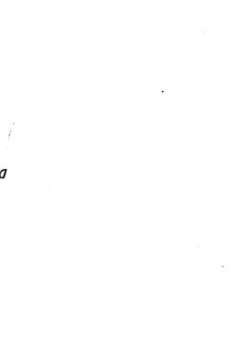
TRAFFIC SIGNAL OPERATION NOTES

- ILLUMINATION ELEMENTS IN THE YELLOW LENSES SHALL BE FLASHING ALTERNATELY AT A RATE OF NOT LESS THAN 50 NOR MORE THAN 60 TIMES PER MINUTE EACH.
- THE ILLUMINATED PERIOD OF EACH FLASH SHALL NOT BE LESS THAN HALF NOR MORE THAN TWO THIRDS OF THE CYCLE.
- WHEN THE SIGNALS ARE FLASHING THE SPEED MESSAGE "15" SHALL BE VISIBLE.
- TIMER SHALL ALLOW THE FLASHERS TO OPERATE AS FOLLOWS:
 - DURING SCHOOL HOURS: REFER TO SHEET 1 OF THE PERMIT.
 - TIMER SHALL BE CAPABLE OF YEARLY PROGRAMMING TO ELIMINATE FLASHING DURING SCHEDULED HOLIDAYS AND SCHOOL CLOSINGS.
 - TIMER SHALL BE CAPABLE OF MANUAL TURN-ON OF THE FLASHER FOR ONE (1) HOUR OF OPERATION UPON ACTIVATION WITH A KEY.
- FEEDBACK DISPLAY OPERATION NOTES:
 - RADAR SPEED DISPLAY TO OPERATE ONLY DURING HOURS OF OPERATION OF THE 15 MPH FLASHERS.
 - PEDESTAL-MOUNTED FLASHER ASSEMBLIES SHALL BE MOUNTED ON A 14-FOOT PEDESTAL POLE.
 - RADAR SPEED DISPLAY SHALL INDICATE SPEED OF VEHICLE AS A STEADY MESSAGE IF VEHICLE IS ABOVE THE POSTED SPEED LIMIT AND BELOW THE COLOR OF THE SPEED DISPLAYED SHALL BE YELLOW.
 - RADAR SPEED DISPLAY SHALL INDICATE SPEED OF VEHICLE AS A FLASHING MESSAGE IF VEHICLE IS AT OR GREATER THAN 1 MPH TO 9 MPH ABOVE THE POSTED SPEED LIMIT. THE COLOR OF THE FLASHING MESSAGE SHALL BE YELLOW.
 - RADAR SPEED DISPLAY SHALL INDICATE SPEED OF VEHICLE AS A FLASHING MESSAGE AND MAY BE FOLLOWED BY A FLASHING "SLOW DOWN" MESSAGE IF THE VEHICLE IS FLASHING MESSAGE SHALL BE YELLOW.
 - ALL SPEEDS AND MESSAGES ON THE RADAR SPEED DISPLAY SHALL BE ON A BLACK BACKGROUND.
 - FLASHING SPEEDS AND MESSAGES SHALL FLASH AT A RATE OF 50 TO 60 CYCLES PER MINUTE.
 - WHEN NO TRAFFIC IS DETECTED THE DISPLAY SHALL BE BLANK (BLACK BACKGROUND).
 - REQUEST TO MODIFY THE RADAR SPEED SIGN, INCLUDING BUT NOT LIMITED TO THE OPERATION AND MESSAGING, AND/OR TO RELOCATE SHALL BE APPROVED BY THE DEPARTMENT.
 - CONTACT PENNDOT TRAFFIC UNIT FOR REQUESTS TO CHANGE THE POSTED SPEED LIMITS ALONG A STATE ROADWAY IF DIFFERENT FROM REQUESTS TO BE DISPLAYED ON SPEED LIMIT SIGN AND RELATED SPEED LIMIT SIGNS.



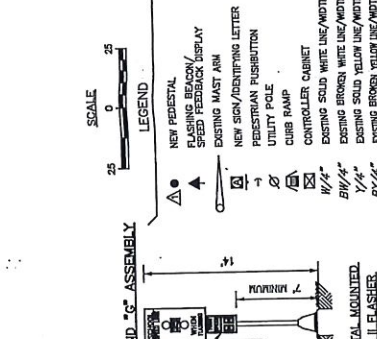
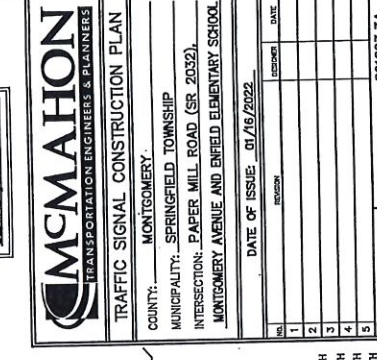
CONTRACTOR NOTES

- THE TOWNSHIP MUST BE MADE AWARE OF AND BE PRESENT AT THE PRE-CONSTRUCTION MEETING AND POLE SPOT MEETINGS WITH PENNDOT.
- THE TOWNSHIP MUST BE PROVIDED ALL SHOP DRAWINGS AND CATALOG CUTS FOR THE TOWNSHIP PERMIT.
- THE TOWNSHIP MUST BE GIVEN THE CONSTRUCTION SCHEDULE AND BE NOTIFIED 24 HOURS PRIOR TO WORK COMMENCING SO THEY MAY BE PRESENT FOR ANY SIGNIFICANT CONSTRUCTION (FOUNDATION POURING, CONDUIT TRENCHING, ETC.) AT THE INTERSECTION.



CONTRACTOR NOTES

- THE TOWNSHIP MUST BE MADE AWARE OF AND BE PRESENT AT THE PRE-CONSTRUCTION MEETING AND POLE SPOT MEETINGS WITH PENNDOT.
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14 SEVEN SEVENBY HIGH VLY SIGNS SHALL NOT BE USED FOR SPEED ENFORCEMENT PURPOSES.

TRAFFIC SIGNAL NOTES

DO NOT MODIFY INSTALLATION WITHOUT PRIOR WRITTEN APPROVAL. ALL SIGNS AND PAVEMENT MARKINGS INDICATED ARE PART OF THE PERMIT. INSTALL AND MAINTAIN IN ACCORDANCE WITH PUBLIC WORKS 212 AND 213. ALL SIGNS SHALL BE MAINTAINED WITH PROPER CLEARANCE AND 8 FEET ABOVE SIDEWALK OR PAVEMENT GRADE.

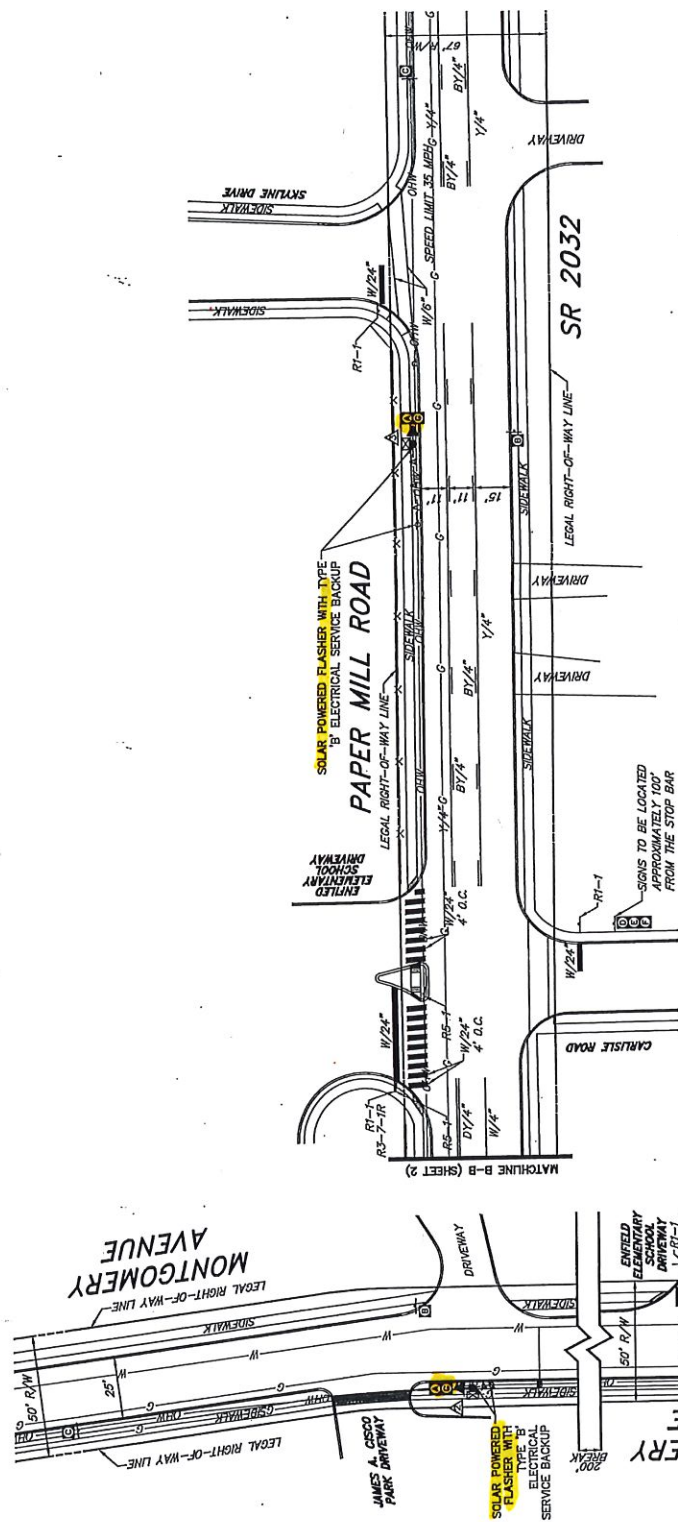
CROSSROAD SIGNALS:
 ALL SIGNALS SHALL BE MAINTAINED WITH PROPER CLEARANCE OF 16 FEET MINIMUM AND 18 FEET MAXIMUM ABOVE ROADWAY. SIGNALS SHALL BE MAINTAINED WITH PROPER CLEARANCE OF 16 FEET MINIMUM AND 18 FEET MAXIMUM ABOVE SIDEWALK. SIGNALS SHALL BE MAINTAINED WITH PROPER CLEARANCE OF 16 FEET MINIMUM AND 18 FEET MAXIMUM ABOVE SIDEWALK. SIGNALS SHALL BE MAINTAINED WITH PROPER CLEARANCE OF 16 FEET MINIMUM AND 18 FEET MAXIMUM ABOVE SIDEWALK.

DETERMINE WITH A PENNOD REPRESENTATIVE, THE EXACT LOCATION OF DETECTORS PRIOR TO INSTALLATION. ALL SIGNS AND UTILITIES TO RESOLVE CONFLICTS PRIOR TO CONSTRUCTION.

COMPLY WITH PROVISIONS OF ACT 287 FOR PREVENTION OF DAMAGE TO UNDERGROUND UTILITIES, DATED 1974.

ALL SIGNS SHALL BE MAINTAINED AND MAINTAINED FROM THE ORIGINAL SPECIFICATION.

THIS CONSTRUCTION PLAN WAS PREPARED FROM THE ORIGINAL PENNOD PERMIT PLAN. COPIES OF THE SIGNED PERMIT CAN BE OBTAINED FROM THE PENNOD REPRESENTATIVE. PENNOD PERMIT NUMBER: SE-247-02.



SCALE

LEGEND

- NEW PAVED
- FLASHING BEACON
- SPEED FEEDBACK DISPLAY
- EXISTING WAST ARM
- NEW SIGN/RESIDING LETTER
- PEDESTRIAN PUSHBUTTON
- UTILITY POLE
- CURB RAMP
- CONTROLLER CABINET
- EXISTING SOLID WHITE LINE/WIDTH
- EXISTING BROKEN WHITE LINE/WIDTH
- EXISTING SOLID YELLOW LINE/WIDTH
- EXISTING BROKEN YELLOW LINE/WIDTH

TRAFFIC SIGNAL CONSTRUCTION PLAN

COUNTY: MONTGOMERY

MUNICIPALITY: SPRINGFIELD TOWNSHIP

INTERSECTION: PAPER MILL ROAD (SR 2032), MONTGOMERY AVENUE AND ENFIELD ELEMENTARY SCHOOL

DATE OF ISSUE: 07/16/2022

NO.	REVISION	DATE
1		
2		
3		
4		
5		



- A - Current Trail to Oreland Neighborhood
- B - Upper Dublin Trail System Township Trail Connectivity
- C - Septa Oreland Train Station

Bridge to Link

DATE: _____

CONSULTANT:
MSP ENGINEERS

STRUCTURAL ENGINEER:

ISSUED DESIGNER:

REVISIONS:

No.	Description	Date

RIKA

LANCE R. RANKINS ASSOCIATES
Architecture - Interior - Design Consulting
750 Forest Avenue
Riverside, Pennsylvania 17080
Tel: 717.576.9899
www.rikainc.com

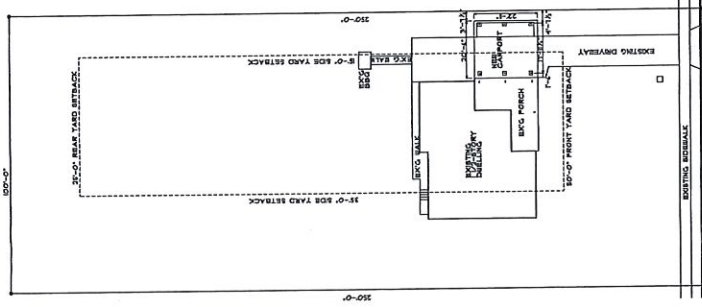
PROJECT INFORMATION:
Project No. 2102
Proposed Carport Addition
300 Cobden Road
Loverock, PA 17026

Client:
Mr. and Mrs. Nelsley Henry
300 Cobden Road
Loverock, PA 17026

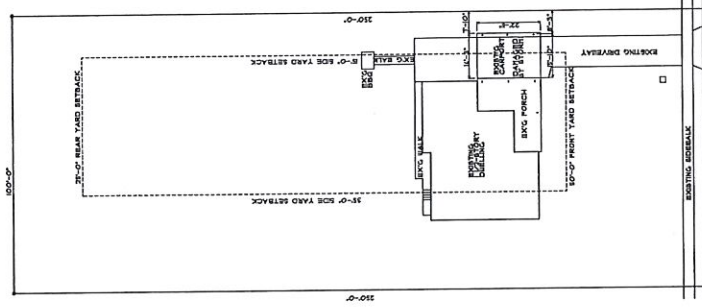
Sheet Title:
Site Plans, Location Plan, Proposed
Plan, Elevation, Section

Sheet:
A1

Date:
14 January 2022



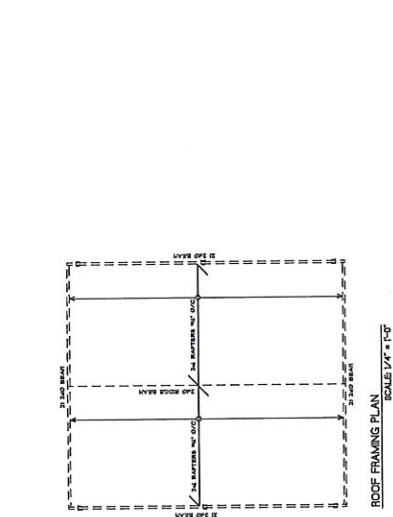
PROPOSED SITE PLAN
SCALE 1/400'-0"



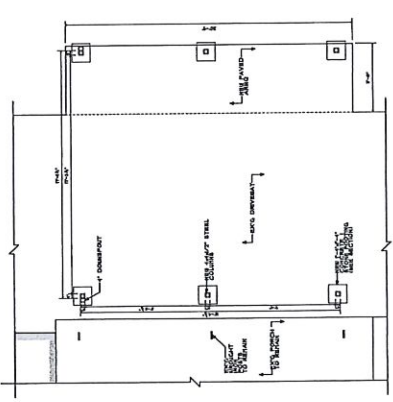
EXISTING SITE PLAN
SCALE 1/400'-0"



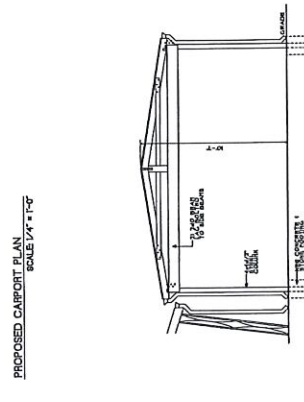
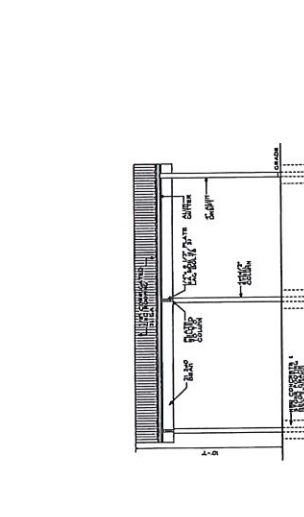
LOCATION PLAN
SCALE 1/250'-0"



PROPOSED CARPORT PLAN
SCALE 1/4" = 1'-0"

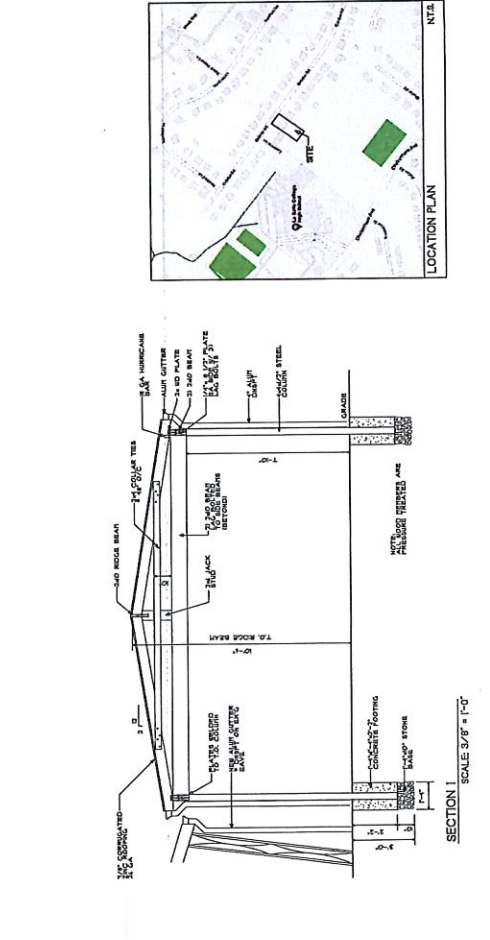


ROOF FRAMING PLAN
SCALE 1/4" = 1'-0"



PROPOSED FRONT ELEVATION
SCALE 1/4" = 1'-0"

PROPOSED RIGHT ELEVATION
SCALE 1/4" = 1'-0"



SECTION I
SCALE 3/8" = 1'-0"

TOWNSHIP OF SPRINGFIELD
MONTGOMERY COUNTY
1510 PAPER MILL ROAD
WYNDMOOR, PA 19038

NO. 22-03

DATE: 1/28/2022

PETITION

SPRINGFIELD TOWNSHIP ZONING HEARING BOARD

We William R. Murphy and Cathy R. Murphy
(Name of Applicant)

Of (Address) 2106 Carlton Way, Flourtown, PA 19031

(Telephone No.) 215-836-0411

do hereby make application before the Springfield Township Zoning Hearing Board to request:

X An **appeal** from the decision of the Zoning/Building Official.

 A **special exception** as provided for in Article , Section ,
Subsection , of the Springfield Township Zoning Code.

X A **variance** from the requirements set forth in Article V, Section 54,
Subsection A(1), of the Springfield Township Zoning Code. X11B 12 B3
A

 Other (please specify)

The property concerned is located at Carlton Way; tax parcel no. 52-00-03025-01-9
Block 48A, Unit 27

Petitioner's Interest in the property is legal owner

Present use of property vacant lot

Explanation of Petition: Variance and Special Exception Requests must meet the statutory guidelines Outlined in Section 114-165 of the Township Zoning Code. The following explanation should indicate Compliance with those guidelines.

See attached addendum

APPLICANT NOTE: Petition must be accompanied by eight (8) sets of scaled drawings or plans, Application Fee and a copy of the property deed

check # 249
#500.00

Cathy Restitutti
Applicant's Signature
Cathy Restitutti
Owner's Signature

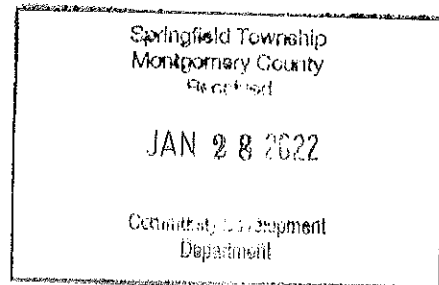
Do not write in this space.

Petition granted.

Petition refused.

The following special conditions are imposed.

By Order of the Zoning Hearing Board



**SPRINGFIELD TOWNSHIP ZONING HEARING BOARD
ADDENDUM TO PETITION**

Petitioners/Owners: William J. Murphy and Cathy R. Murphy
2106 Carolton Way
Flourtown, PA 19031

Property: Carolton Way
TPN: 52-00-03025-01-9
Block 48A, Unit 27

- Requested Relief:
1. An appeal of the Zoning Officer's determination that section 114-12B3.A "Regulations for Areas of Steep Slopes" is applicable to the development of the Property based on the approved and recorded subdivision plan, or in the alternative a variance from section 114-12B3.A to permit more than 40% of the total lot area to be regraded.

 2. A variance from section 114-54.A(1) "Front yards".

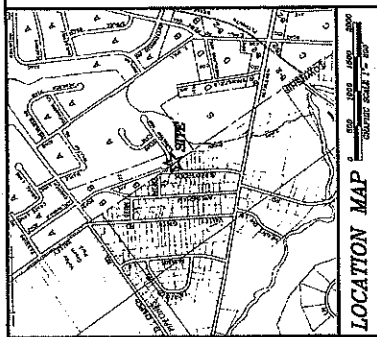
To permit a 30' front yard setback rather than the required 40'.

INTRODUCTION

This petition proposes the construction of single family dwelling with attached garage and deck, which is permitted by right in the A Residential District, on an approved subdivided lot.

Petitioners/Owners William J. Murphy and Cathy R. Murphy ("Landowners") are the legal owners of a vacant parcel of land located on Carolton Way, Springfield Township ("Township"), Pennsylvania, and identified as Montgomery County tax parcel number 52-00-03025-01-9 ("Property").

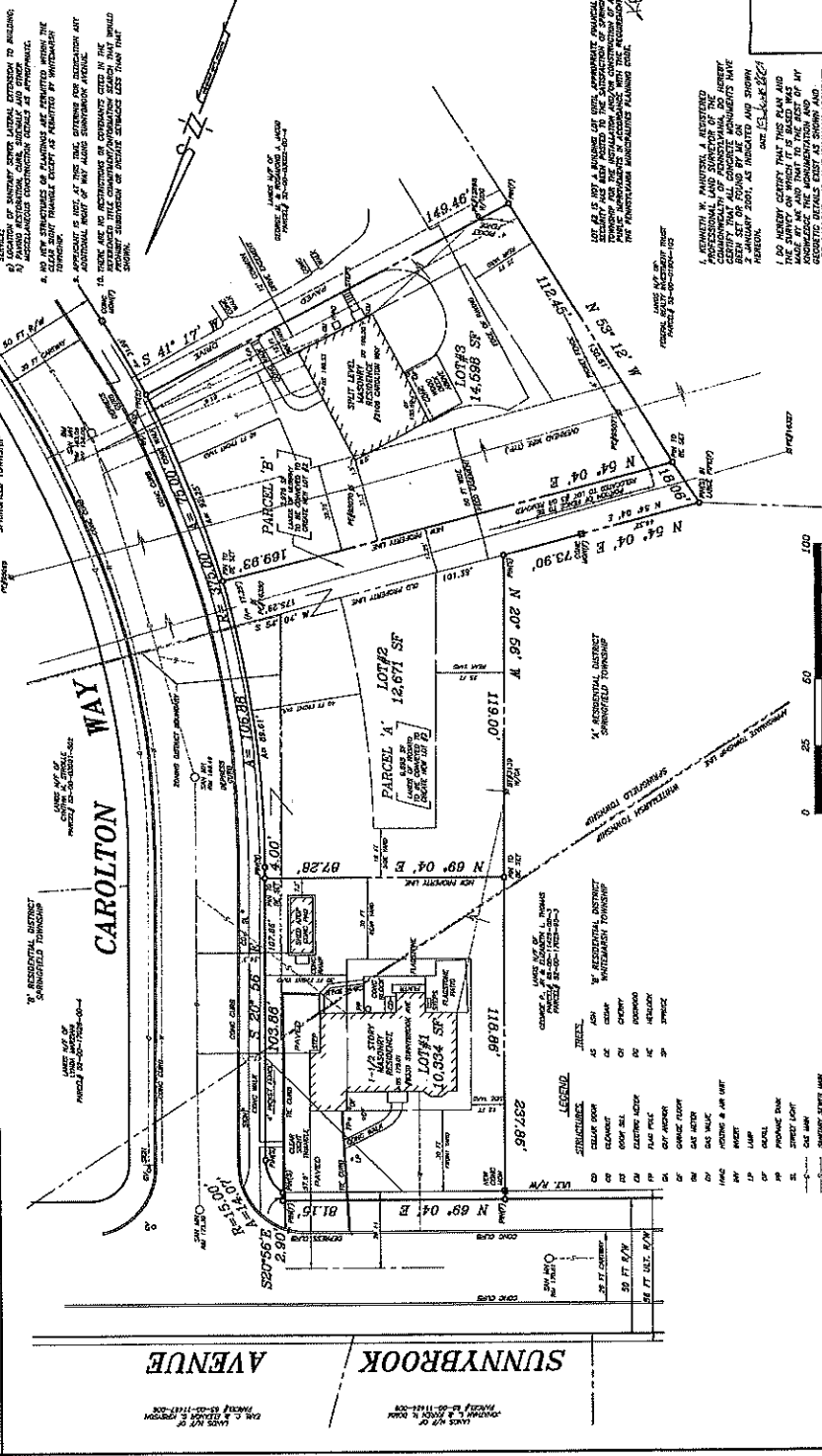
EXHIBIT “A”



LOCATION MAP

SUBDIVISION NOTES table with columns for lot numbers, areas, and descriptions.

SURVEY NOTES detailing the survey process, including references to previous surveys and the location map.



GRAPHIC SCALE 1" = 20'

COMMUNITY OF PENNSYLVANIA THOMAS & CATHERINE F. ROSATO THOMAS & CATHERINE F. ROSATO

WILLIAM J. & CATHY MURPHY WILLIAM J. & CATHY MURPHY

SUBDIVISION PLAN THOMAS & CATHERINE ROSATO WILLIAM & CATHY MURPHY SPRINGFIELD TOWNSHIP MONTGOMERY COUNTY PENNSYLVANIA

PARCELS 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100

LEGEND

SYMBOLS	
CL	CLEARANCE
CO	CONCRETE
CS	CURB
CB	BRICK
CL	CLAY
DI	DIAMOND
DS	DRAINAGE
FM	FIRE
GM	GROUNDED
HE	HANDICAP
HT	HATCH
IC	IRON
IS	ISLAND
LI	LANDSCAPE
LV	LANDSCAPE
MS	MASONRY
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE
OS	ORANGE

NOTES:

- LOT AREA = 30,829 S.F. (AS PER CAROLTON PLAN)
- LOT AREA = 17,874 S.F. (AS PER CAROLTON PLAN)
- CONTRACTORS TO BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES.
- CONTRACTORS TO BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES.
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- CONTRACTORS TO BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES.

EXISTING FEATURES PLAN

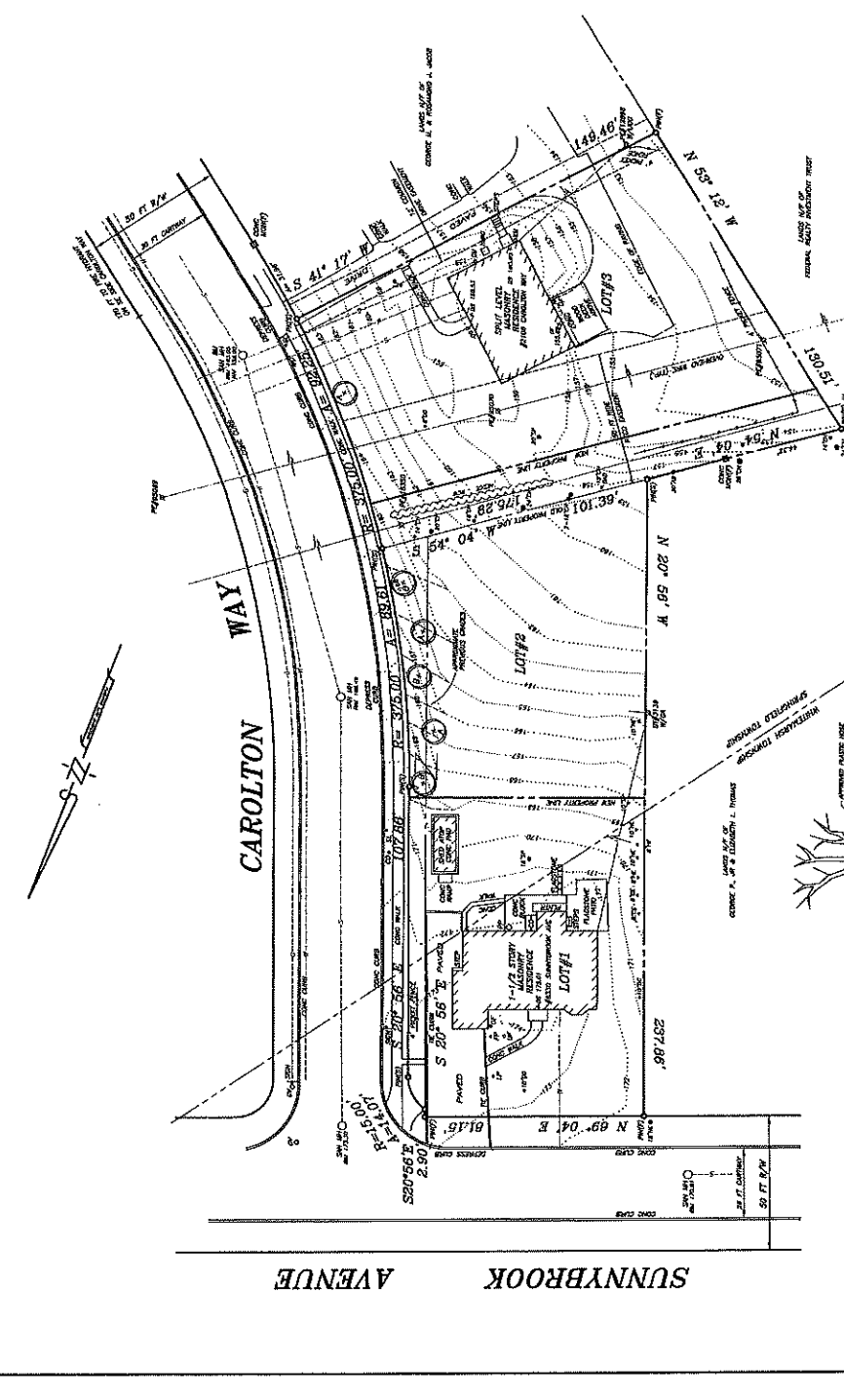
THOMAS & CATHERINE ROSATO
WILLIAM & CATHY MURPHY
SPRINGFIELD TOWNSHIP
MONTGOMERY COUNTY
PENNSYLVANIA

PROJECT NO. 2 OF 2

PREPARED BY
PARUTSKI LAND SURVEYING
500 BRIDGEBEND ROAD
OAKLAND, PENNSYLVANIA 15075
(412) 233-9826, FAX 412-233-9845

DATE: 2 JAN 2011
PLAN NO.: 21-02-10-19

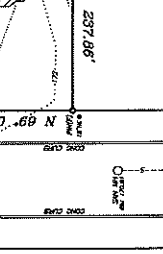
SCALE: 1" = 20'



PROPOSED PLANTINGS LEGEND

ACT	QTY	SIZE	PLANT LIST	ESPANISH NAME	COMMON NAME
A	3	2 1/2" - 3" CAL	QUERCUS ALBA		WHITE OAK
B	3	2 1/2" - 3" CAL	FRAXINUS AMERICANA		WHITE ASH

STREET PLANTINGS SHALL BE SPACED AT LEAST 10 FEET FROM THE CURB AND 10 FEET FROM THE PROPERTY LINE. ALL PLANTINGS SHALL BE INSTALLED IN ACCORDANCE WITH THE MOUNTAIN STATES ASSOCIATION OF LANDSCAPE ARCHITECTS (MSLA) STANDARD SPECIFICATIONS FOR PLANTINGS IN URBAN SETTINGS.



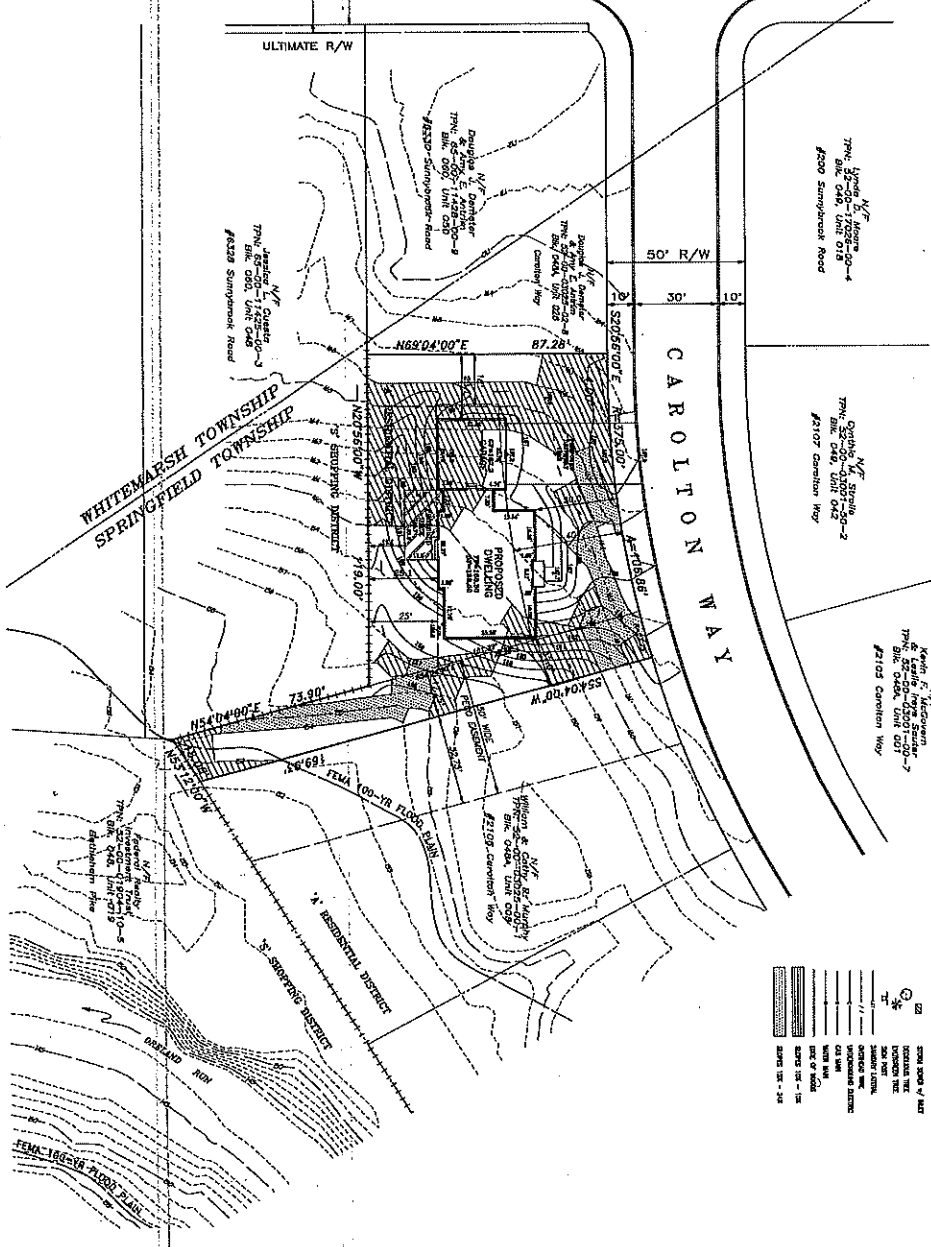
0 20 40 100
GRAPHIC SCALE 1" = 20'

EXHIBIT “B”



SUNNYBROOK ROAD

CAROLTON WAY



SOIL CLASSIFICATION AS SHOWN FROM THE
STATE OF PENNSYLVANIA
OFFICE OF STATE ENGINEERS
DIVISION OF HIGHWAYS

UNIT 1 - 1/8" = 1'-0" (Vertical)
 UNIT 2 - 1/4" = 1'-0" (Horizontal)

AVERAGE EARTH COVER
 STATE OF PENNSYLVANIA
 OFFICE OF STATE ENGINEERS
 DIVISION OF HIGHWAYS
 1:25.0

GRADES TABLE		
GRADE	FINISHED GRADE	PROPOSED GRADE
1.50	1.50	1.50
1.75	1.75	1.75
2.00	2.00	2.00
2.25	2.25	2.25
2.50	2.50	2.50
2.75	2.75	2.75
3.00	3.00	3.00

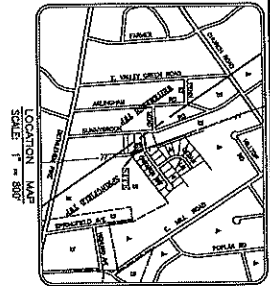
CONCRETE FINISH GRADES		
FINISH GRADE	FINISHED GRADE	PROPOSED GRADE
1.50	1.50	1.50
1.75	1.75	1.75
2.00	2.00	2.00
2.25	2.25	2.25
2.50	2.50	2.50
2.75	2.75	2.75
3.00	3.00	3.00

LEGEND

- EXISTING**
- Property Line
 - Topography
 - Waterway
 - Drainage
 - Utility Lines
 - Proposed Building
 - Proposed Driveway
 - Proposed Parking
 - Proposed Walkway
 - Proposed Sidewalk
 - Proposed Street
 - Proposed Right-of-Way
 - Proposed Easement
 - Proposed Survey
 - Proposed Contour
 - Proposed Spot Elevation
 - Proposed Slope
 - Proposed Area
 - Proposed Volume
- PROPOSED**
- Proposed Building
 - Proposed Driveway
 - Proposed Parking
 - Proposed Walkway
 - Proposed Sidewalk
 - Proposed Street
 - Proposed Right-of-Way
 - Proposed Easement
 - Proposed Survey
 - Proposed Contour
 - Proposed Spot Elevation
 - Proposed Slope
 - Proposed Area
 - Proposed Volume

NOTES

1. Engineer's professional seal and signature shall appear on this plan and the accompanying title block.
2. The owner shall be responsible for obtaining all necessary permits from the appropriate government agencies.
3. The owner shall be responsible for obtaining all necessary easements from the appropriate property owners.
4. The owner shall be responsible for obtaining all necessary approvals from the appropriate zoning board.
5. The owner shall be responsible for obtaining all necessary approvals from the appropriate planning commission.
6. The owner shall be responsible for obtaining all necessary approvals from the appropriate council.



ZONING: 'A' RESIDENTIAL DISTRICT

LOT NO.	AREA, SQ. FT.	REQUIREMENTS	REMARKS
114-001A	12,000 S.F.	1-1/2' MIN. SIDE YARD SETBACK	1-1/2' MIN. SIDE YARD SETBACK
114-001B	12,000 S.F.	1-1/2' MIN. SIDE YARD SETBACK	1-1/2' MIN. SIDE YARD SETBACK
114-001C	12,000 S.F.	1-1/2' MIN. SIDE YARD SETBACK	1-1/2' MIN. SIDE YARD SETBACK

SITE AREA
12,871 S.F. or 0.2939 AC.

ZONING PLAN
CAROLTON WAY
 PREPARED FOR
WILLIAM & CATHY R. MURPHY
 SPRINGFIELD TOWNSHIP, MONTGOMERY COUNTY, PA.

DATE	NO.	REVISION	BY

CHARLES E. SHOEMAKER, INC.
 ENGINEERS & SURVEYORS
 MONTGOMERYVILLE, PA 17056
 PHONE: 717-697-3183 FAX: 717-697-7791
 E-MAIL: CSO@CESHOEMAKER.COM
 SCALE 1" = 20'

RECORD OWNER
WILLIAM MURPHY
& CATHY R. MURPHY
 2108 CAROLTON WAY
 FLOURCOAN, PA 18031

COPYRIGHT © 2005
 BLOCK - 114
 LOT - 114-001A
 THE PROPOSED
 CONSTRUCTION
 PLAN FOR THE
 114-001A
 2108 CAROLTON WAY
 FLOURCOAN, PA 18031

Speed Enforcement Evaluator

Location:
8100 Flourtown Ave

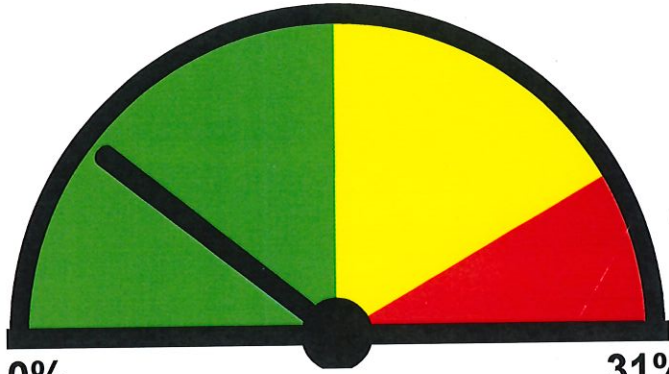
**Total Percentage of
 Enforceable Violations**

Closest Cross Street:
 Gilbert Ave

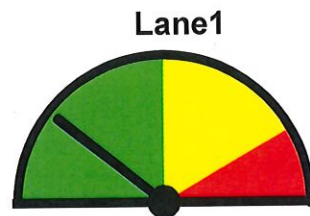
Analysis Dates:
 Monday, January 10, 2022
 Monday, January 17, 2022

Equipment Used:
 JAMAR Radar Recorder

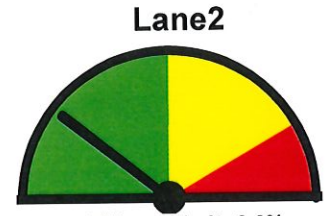
Installed By:
 Sgt. Shawn Hart



0% **31%**
Posted Speed Limit: 25 MPH
Enforcement Tolerance: 10 MPH
Enforcement Limit: 36 MPH & Up
Percentage Above Limit: 6.6%
Enforcement Rating: **LOW**



Percent Above Limit: 6.6%
Enforcement Rating: LOW



Percent Above Limit: 6.6%
Enforcement Rating: LOW

Combined

1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65
0	5	367	1929	3822	3512	1612	485	152	79	35	11	5	27

85 percentile = 31

Lane1

1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65
0	5	119	871	1746	1747	841	249	74	38	15	1	0	2

85 percentile = 31

Lane2

1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65
0	0	248	1058	2076	1765	771	236	78	41	20	10	5	25

85 percentile = 31

Springfield Twp Police

Traffic Safety Div
1510 Paper Mill Road Wyndmoor, PA 19038
215-836-1605

															Lane1
Date\Speed (MPH)	1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65	Total
1/10/2022	0	0	21	91	161	172	112	34	1	1	4	0	0	0	597
1/11/2022	0	2	19	178	331	278	104	42	28	13	6	1	0	0	1002
1/12/2022	0	0	11	142	260	225	102	23	5	4	0	0	0	0	772
1/13/2022	0	1	15	140	247	241	118	35	3	0	0	0	0	1	801
1/14/2022	0	1	11	85	267	305	128	29	2	1	0	0	0	0	829
1/15/2022	0	0	19	85	237	276	173	62	13	8	2	0	0	1	876
1/16/2022	0	0	14	104	211	214	107	26	22	11	2	0	0	0	711
1/17/2022	0	1	19	60	57	56	17	6	0	0	1	0	0	0	217
Lane1 Total	0	5	129	885	1771	1767	861	257	74	38	15	1	0	2	5805

85 percentile = 31

															Lane2
Date\Speed (MPH)	1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65	Total
1/10/2022	0	0	20	108	214	122	34	13	2	2	1	1	2	1	520
1/11/2022	0	0	17	145	342	324	110	54	18	11	0	0	4	2	1027
1/12/2022	0	0	24	153	362	231	91	17	6	3	1	1	0	3	892
1/13/2022	0	0	27	158	312	193	48	4	3	1	0	0	0	1	747
1/14/2022	0	0	20	154	323	260	65	13	2	2	0	1	0	0	840
1/15/2022	0	0	9	89	227	310	218	61	22	11	12	2	0	9	970
1/16/2022	0	0	22	78	178	254	176	65	23	9	3	4	0	5	817
1/17/2022	0	0	111	187	145	101	37	15	4	3	3	2	1	4	613
Lane2 Total	0	0	250	1072	2103	1795	779	242	80	42	20	11	7	25	6426

85 percentile = 31

															Combined
Date\Speed (MPH)	1-5	6-10	11-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	>65	Total
1/10/2022	0	0	41	199	375	294	146	47	3	3	5	1	2	1	1117
1/11/2022	0	2	36	323	673	602	214	96	46	24	6	1	4	2	2029
1/12/2022	0	0	35	295	622	456	193	40	11	7	1	1	0	3	1664
1/13/2022	0	1	42	298	559	434	166	39	6	1	0	0	0	2	1548
1/14/2022	0	1	31	239	590	565	193	42	4	3	0	1	0	0	1669
1/15/2022	0	0	28	174	464	586	391	123	35	19	14	2	0	10	1846
1/16/2022	0	0	36	182	389	468	283	91	45	20	5	4	0	5	1528
1/17/2022	0	1	130	247	202	157	54	21	4	3	4	2	1	4	830
Combined Total	0	5	379	1957	3874	3562	1640	499	154	80	35	12	7	27	12231

85 percentile = 31

BY-LAWS
SPRINGFIELD TOWNSHIP PARKS AND RECREATION ADVISORY COMMITTEE

ARTICLE I NAME OF ORGANIZATION

This organization shall be known as the "Springfield Township Parks and Recreation Advisory Committee".

ARTICLE II PURPOSE OF THE PARKS AND RECREATION ADVISORY COMMITTEE

The purpose of the Parks and Recreation Advisory Committee shall be to recommend to the Board of Commissioners those policies and procedures that will encourage year-round active and passive recreation programs for all Township residents; to coordinate Township recreation activities with those provided by other organizations so as to unify a Township recreation program under the leadership of the Township's Recreation Director.

The Committee shall also oversee park use within the Township as well as recommend new development or improvement to the park areas.

ARTICLE III GOALS AND OBJECTIVES FOR THE RECREATION FUNCTION

The goals and objectives for the recreation function in Springfield Township are as follows:

- A. To make use of all public and private facilities and activities as a means to accomplish the major goal of bringing people together to know and understand each other, and to develop a healthy union of mind and body; and
- B. To operate, to the extent possible, recreation programs on a self-sustained basis; and
- C. To coordinate the use of Township-owned play areas with play fields and school facilities owned by the School District in order to maximize the use of publicly-owned property that is adaptable for recreational use.

ARTICLE IV MEMBERSHIP OF THE PARKS AND RECREATION ADVISORY COMMITTEE

Section 1. The Parks and Recreation Advisory Committee shall be composed of seven (7) members, two of whom shall be appointed by the School Board of the School District of Springfield Township, one of whom shall be a School Board Member. The remaining five (5) members shall be appointed by the Board of Commissioners. The Recreation Director shall serve as the Secretary and a non-voting member. The Board of Commissioners shall appoint a Commissioner as a liaison to the Recreation Advisory Committee. The Commissioner is a non-voting participant.

Section 2. The term of office for each members shall be three (3) years or until his/her successor is appointed and qualifies; except for the initial appointment which shall be as follows: three appointees for one year; three appointees for two years; and three appointees for three years. The Members of the Committee shall serve without pay and shall serve their full term, subject to the right to voluntarily resign, or until a member shall be removed by the Board of Commissioners. Any member missing three consecutive meetings, without valid reasons or proper notification, shall be subject to removal from the Committee. It is the responsibility of the Township Commissioner liaison to draw this to the attention of the Board of Commissioners. The Board of Commissioners shall determine whether the member should be replaced.

Section 3. The Recreation Advisory Committee shall meet in regular monthly meetings on the first Wednesday of each month during the year except July and August. Special meetings shall be called by the Chairperson or upon the written request of at least two members. All regular meetings are to be held at the Township Building unless otherwise designated by the Chairperson. The meetings shall convene at 7:30 P.M. unless otherwise ordered. All meetings shall be open to the public.

Section 4. The Chairperson and Vice-Chairperson shall be elected by ballot. Nominations shall be made from the floor. The majority vote of those present shall constitute an election. The purpose of the February meeting shall be to elect a Chairperson and Vice-Chairperson, the Recreation Director shall serve as the Secretary. Four (4) members constitute a quorum at any regular or special meeting. No "proxy" votes are permitted. Simple majority votes shall carry agency motions.

Section 5. The Chairperson shall preside at all meetings of the Committee and represent the Committee at all public functions. The Chairperson shall, with the assistance of the Recreation Director, prepare the regular monthly and special agenda for Committee meetings. The Vice-Chairperson shall perform the duties of the Chairperson in his absence. The Secretary shall keep a record of the proceedings of the Committee and shall forward all communications to the Committee, write correspondence as directed by the Committee, and keep on file all important letters and replies thereto.

Section 6. Members of the Parks and Recreation Advisory Committee may make an inspection of the recreation facilities, functions, and equipment at any time and bring a report before the Committee.

Section 7. Special Committees, as well as Sub-Committees, may be appointed by the Chairperson. The Chairperson and the Recreation Director will be ex-officio members of each such committee.

Section 8. The Board of Commissioners shall appoint one representative of the Friends of Hillcrest Park to serve as an advisor concerning issues related to Hillcrest Park. The Hillcrest Park representative may place items on the agenda relating to the park and may vote only on issues affecting the park. The term shall be for three years from the date of appointment.

ARTICLE V AMENDMENTS TO BY-LAWS

Section 1. The Committee shall reorganize and review the By-Laws annually, preferably in February.

Section 2. These By-Laws may be recommended to be amended at any regular or special meeting of the Parks and Recreation Advisory Committee by a two-thirds vote of the voting members, provided the proposed amendment has been given in writing to all members at least seven days prior to the said meeting. The By-Laws of the Parks and Recreation Advisory Committee, together with any of its proposed amendments, shall be approved by Resolution of the Board of Commissioners.

ADOPTED by the Board of Commissioners on April 10, 1996.

prbylaws

Chapter 59. Parks and Recreation

Article II. Recreation Department; Park and Recreation Committee

§ 59-7. Park and Recreation Committee.

A Park and Recreation Committee, comprised of seven members, is hereby established by the Board of Commissioners to serve in an advisory capacity in accordance with the following duties:

- A. Recommend to the Board of Commissioners the overall philosophy and goals of the park and recreation program.
- B. Recommend to the Board of Commissioners the annual objectives of the park and recreation program within the philosophy and goals already established by the Board of Commissioners.
- C. Recommend to the Board of Commissioners the establishment of policies under which the Recreation Department should operate, including such areas as programs to be conducted, user fees and charges, recreation facilities and all aspects of the park and recreation program.
- D. Elect its officers and adopt a constitution and bylaws for Committee operation.
- E. Serve as a sounding board for the recreational needs of township residents and transmit to the Board of Commissioners for Board approval those plans not previously approved by the Board.
- F. Investigate possible revenue sources to fund recreation programs.
- G. Meet regularly to conduct its work, keep minutes of these meetings and transmit the same to the Board of Commissioners and Township Manager.

RESOLUTION NO. 1562

**A Resolution Authorizing Application to the 2022 Round of the
Montco 2040 Implementation Grant Program for the Cisco Park Improvement and
Phase I of the 1725 Walnut Avenue Park Redevelopment Projects**

WHEREAS, Montgomery County has established the Montco 2040 Implementation Grant Program as a competitive funding program to assist municipalities in implementing the goals of the county comprehensive plan, Montco 2040: A Shared Vision; and

WHEREAS, the County is accepting applications for projects that advance specific goals under either of the county comprehensive plan's three themes: Connecting Communities, Sustainable Places, and a Vibrant Economy; and

WHEREAS, applications and projects must meet all stated requirements within the Montco 2040 Implementation Grant Program Guidebook; and

WHEREAS, the Township of Springfield wishes to obtain \$130,000 from the Montco 2040 Implementation Grant program to provide funding for Park Improvements at Cisco Park in Erdenheim with a local match of up to \$23,000 after utilizing previously awarded state grants totaling \$209,500; and

WHEREAS, the Township of Springfield wishes to submit a second request in order to obtain \$200,000 from the Montco 2040 Implementation Grant program to provide funding for Phase I of the 1725 Walnut Avenue Park Redevelopment project in order to abate and demolish the existing on-site structures. If awarded, the local match to the Township would be \$83,515 after utilizing a previously awarded Community Development Block Grant in the amount of \$107,400.

NOW, THEREFORE, BE IT RESOLVED: that the Board of Commissioners of Springfield Township, Montgomery County, Pennsylvania, hereby approves these projects and authorizes the submission of both application requesting funding from the 2022 round of the Montco 2040 Implementation Grant Program.

UNANIMOUSLY ADOPTED this 9th day of February, 2022.

BOARD OF COMMISSIONERS OF
SPRINGFIELD TOWNSHIP

By: _____
Eddie T. Graham, Esq., President

Attest: _____
A. Michael Taylor, Secretary

Appendix A: Bysheer Field

(Yellow lines indicate Little League property boundaries. Blue lines indicate Township property boundaries.)



Appendix B: Marlow Field

(Yellow lines indicate Little League property boundaries. Blue lines indicate Township property boundaries.)



Appendix C: Springfield Little League Baseball Boundaries (as of 1-1-2022)



Little League Baseball Scoreboard Relocation Request 2022

Springfield Little League would like to repurpose this scoreboard for use at Bysher field. The two areas marked in red are near a power supply and would make the installation easier. The sponsor panels will remain the same size and will be limited to (2) panels. The League name will be updated to reflect Springfield Little League.

Marlow Field Location:



Bysher Field: Proposed Location

